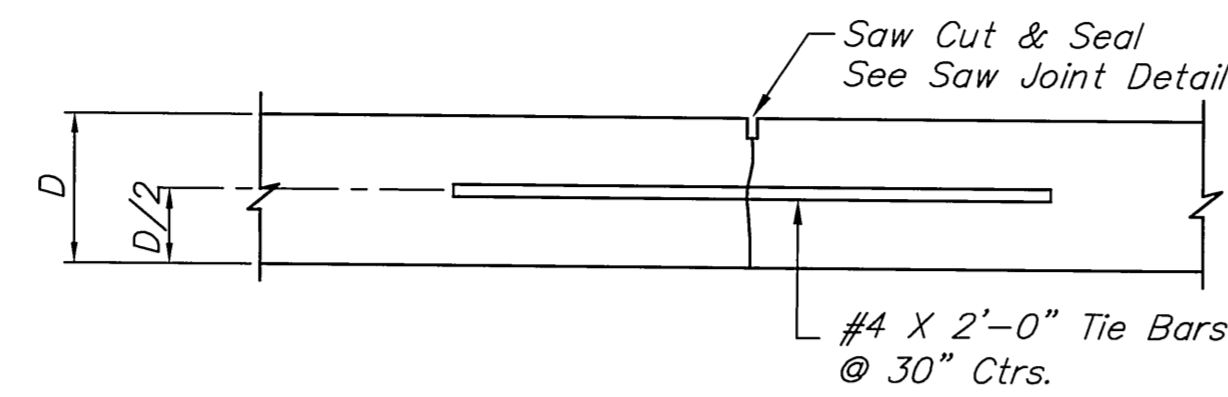
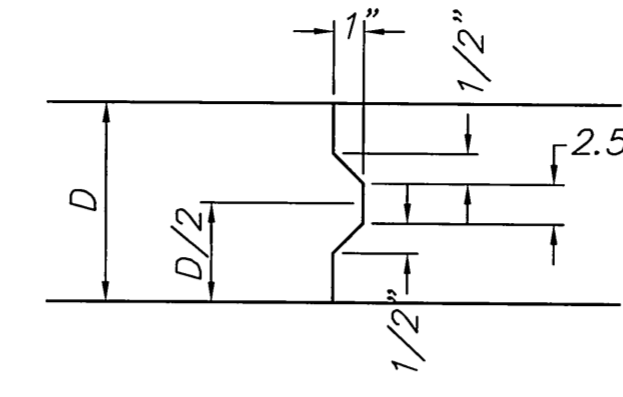


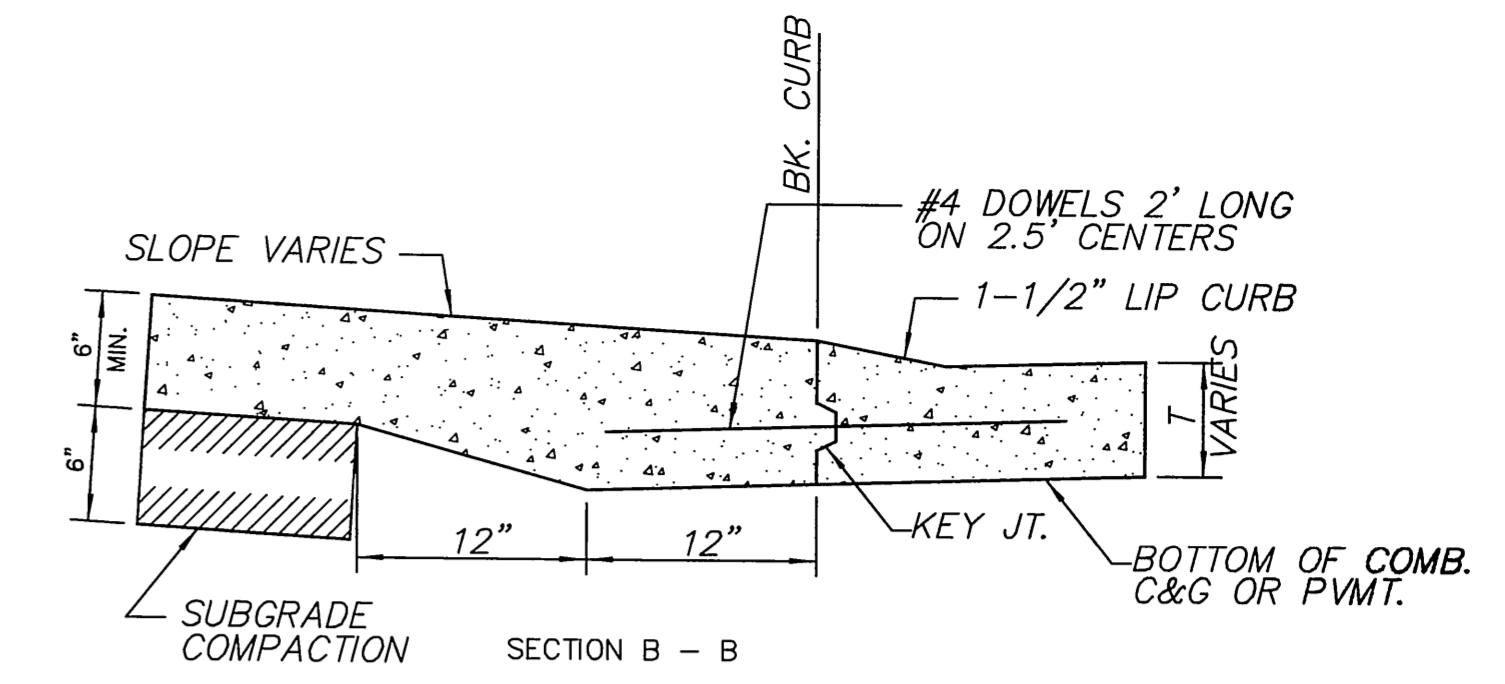
ISOLATION JOINT (I.J.)
(At Structures)



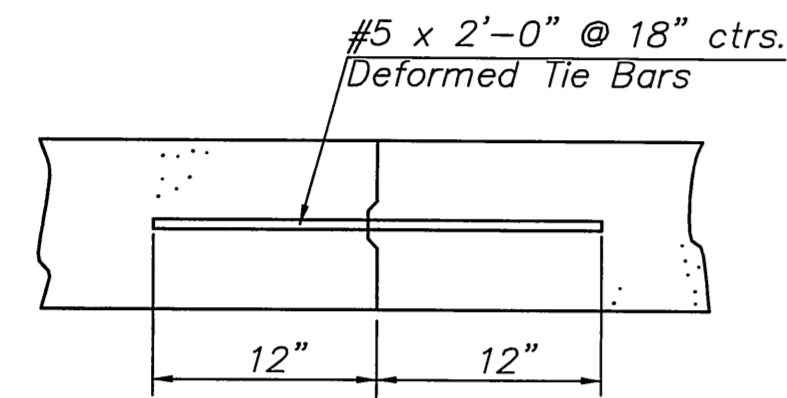
LONGITUDINAL JOINT DETAIL (L.J.)



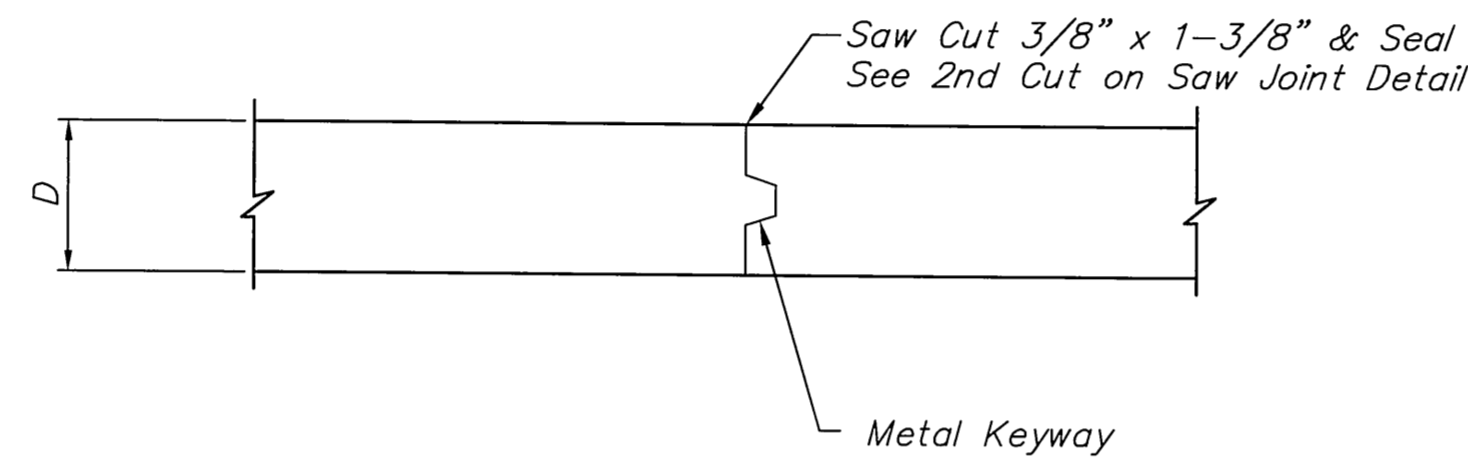
KEYWAY DETAIL



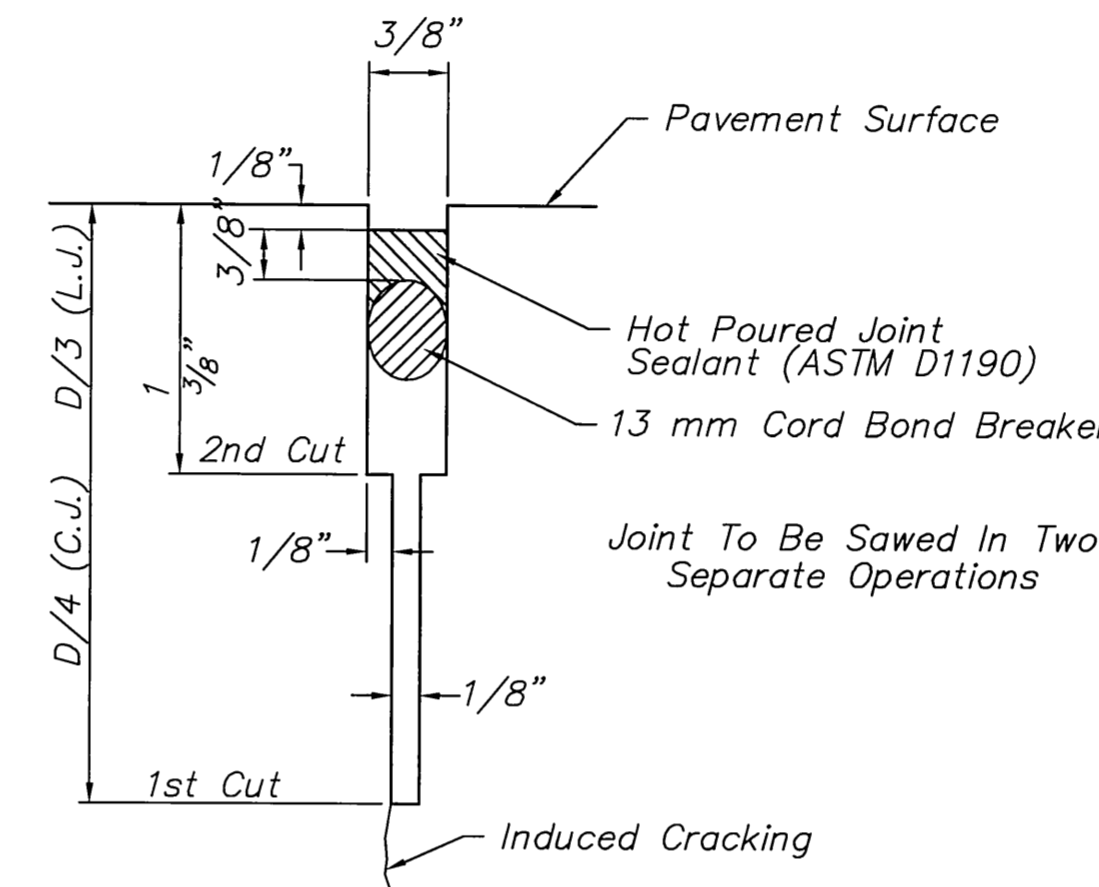
BACK OF CURB DETAIL



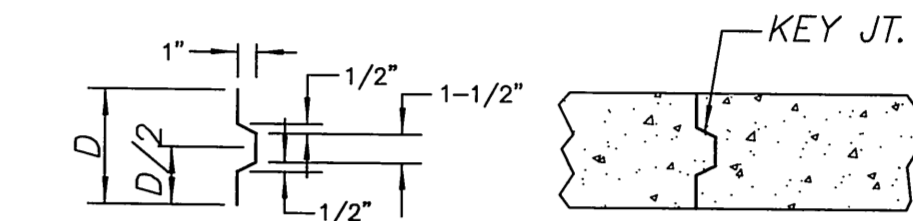
TIED CONSTRUCTION (T.J.) AND CONTRACTION (C.J.) JOINT DETAIL



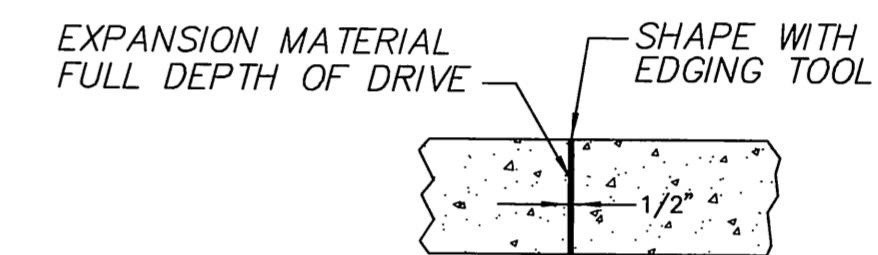
CENTER LINE JOINT DETAIL (C.L.J.)



SAW JOINT DETAIL



ALT. LONGITUDINAL CONSTRUCTION JOINT



EXPANSION JOINT (E.J.)



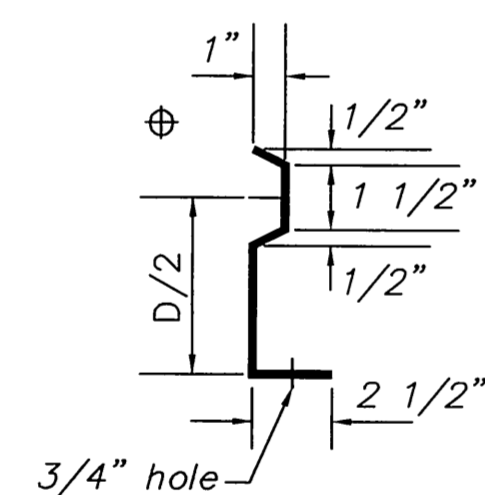
CONTRACTION JOINT (C.J.) OR LONGITUDINAL JOINT (L.J.)

NO SAWN JOINTS WILL BE ALLOWED

GENERAL NOTES

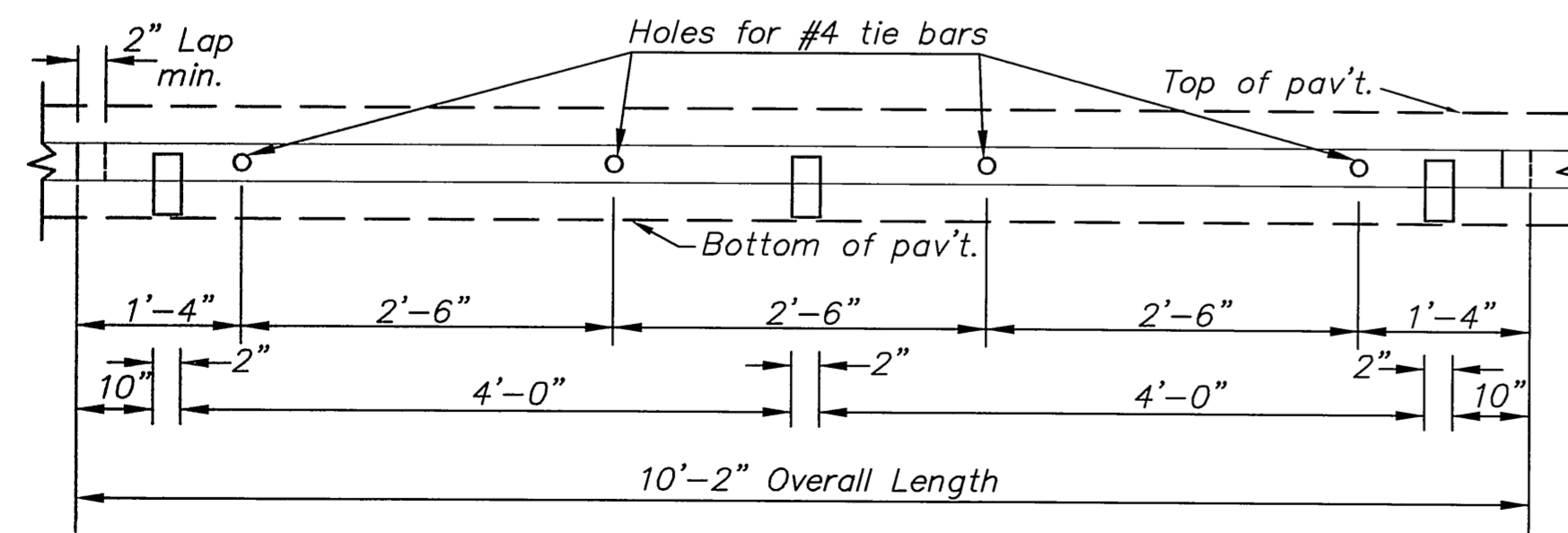
- DRIVEWAY CONSTRUCTION DETAILED ON THIS SHEET IS FOR USE WITH FULL HEIGHT STREET CURBS AND IN AREAS WITHOUT FULL WALK CONSTRUCTION IN THE PARKING. SEE OTHER DETAIL SHEETS FOR DRIVEWAY CONSTRUCTION WITH ROLL CURB AND/OR FULL WALK.
- ONE LONGITUDINAL JOINT SHALL BE CONSTRUCTED ALONG THE CENTERLINE OF DRIVES HAVING A "W" DIMENSION OF 24' OR LESS. TWO LONGITUDINAL JOINTS SHALL BE CONSTRUCTED WITH EQUAL SPACINGS NOT TO EXCEED 10' FOR DRIVES WITH A "W" DIMENSION GREATER THAN 24'.
- DRIVEWAY WIDTH DENOTED AS "W" ON THE DETAIL DRAWINGS SHALL BE A MINIMUM OF 10' AND A MAXIMUM OF 30'. THE MAXIMUM OPENING FOR RADIUS TYPE DRIVES WITH CURBS THROUGH THE RADIUS SHALL NOT EXCEED 52' AT THE STREET CURB LINE.
- CONTRACTION JOINT SPACING IN THE DRIVEWAY WALK SECTION SHALL BE A MINIMUM OF 3' AND A MAXIMUM OF 6' AND ARE TO BE EQUALLY SPACED WITHIN THIS RANGE. WALK SECTION SHALL BE CONSTRUCTED TO THE SAME THICKNESS AS THE DRIVEWAY.
- DOWEL BARS SHALL BE OMITTED FROM THE KEYED CONSTRUCTION JOINT ALONG THE BACK OF THE STREET CURB LINE WHEN DRIVEWAYS ARE CONSTRUCTED IN CONJUNCTION WITH NEW CONCRETE PAVEMENT CONSTRUCTION.
- ADDITIONAL THICKNESS OF DRIVE AS INDICATED IN THE DRAWINGS WILL NOT BE PAID FOR DIRECTLY AND THIS COST SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE DRIVEWAY CONSTRUCTION.
- ONE HALF INCH EXPANSION JOINTS SHALL BE INSTALLED WHEREVER DRIVE CONSTRUCTION ABUTS SIDEWALK. ONE HALF INCH EXPANSION JOINTS SHALL ALSO BE INSTALLED ALONG THE PROPERTY LINE AND/OR BACK OF WALK LINE WHEN DRIVE CONSTRUCTION ALONG THIS LINE ABUTS CONCRETE PARKING LOTS OR CONCRETE DRIVE EXTENSION.
- ALL DRIVEWAYS SHALL BE A MINIMUM OF 8" IN THICKNESS AND SHALL BE REINFORCED WITH 6"x12" W4-W4 WELDED WIRE FABRIC.

Note: Contraction joints will be constructed at the planned location or as directed by the Engineer. When necessary to interrupt continuous placement for a substantial length of time or at the end of a day's pour, the Contractor has the option of ending placement at a contraction joint or with a construction joint located a minimum of five (5) feet from a contraction joint. Either joint type may be constructed by placing a header at the end of the pour or by paving past the joint location, sawing the joint after the concrete has hardened, and drilling holes for the tie bars and securing into the concrete with epoxy or cement grout.



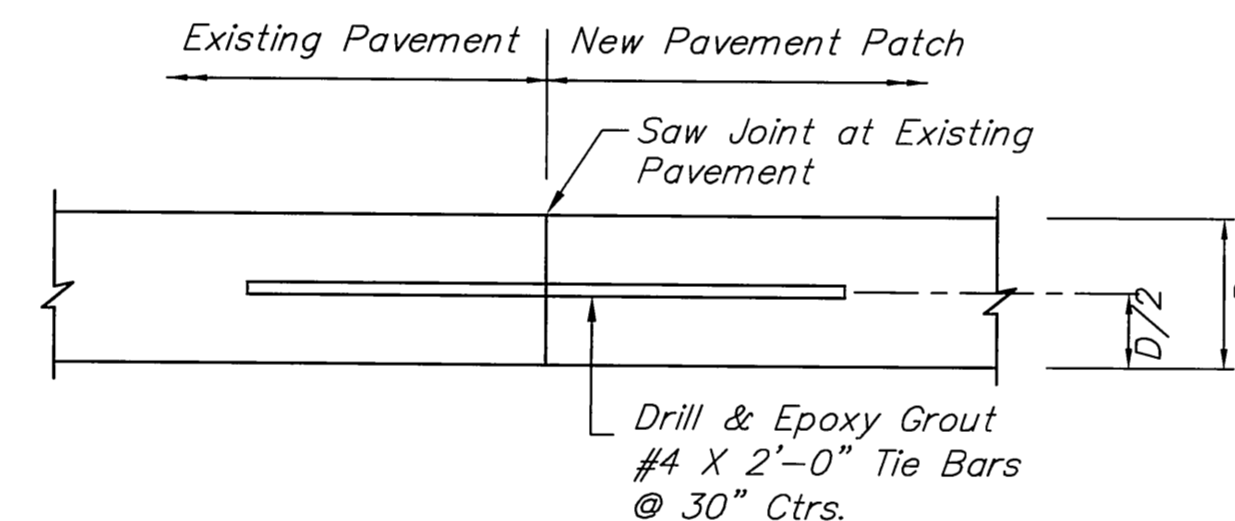
SECTION OF RECESSED FORM LEG

⊕ Snap-in leg or other approved designs may be used in lieu of welded leg.



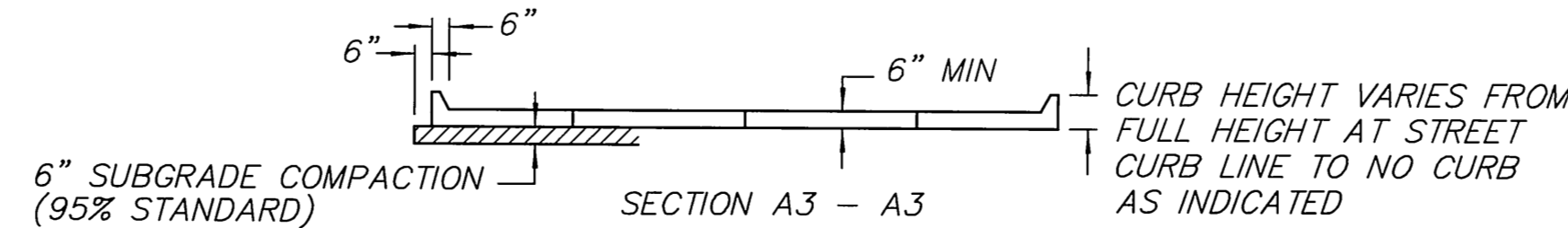
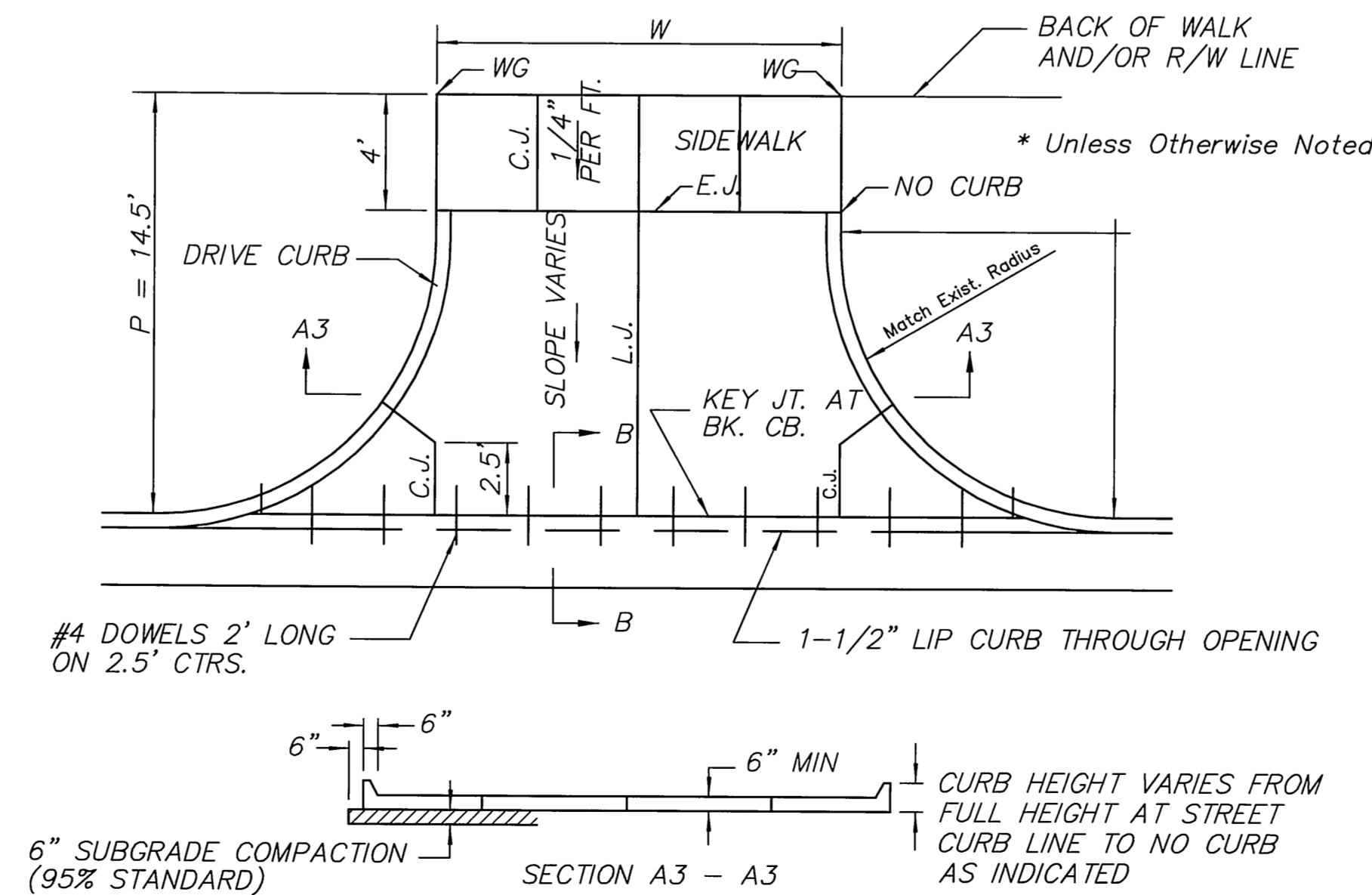
METAL STRIP FOR LONGITUDINAL CONSTRUCTION JOINT

To be used only against forms. Shall not extend through contraction or expansion joints. Other types of construction shall be permitted with the approval of the engineer.



ABUTTING EXISTING PAVEMENT*

* If Joint is at ϕ , Omit Tie Bar.



3RD STREET WATER MAIN REPLACEMENT FROM CENTRAL CORRIDOR TO I-135

PAVEMENT DETAILS

SHEET TITLE
448-90217
PROJECT NUMBER

JRA, JSB, JRA
DESIGNED DRAWN CHECKED

ISSUED
June 2012
REVISED