

PROJECT BENCHMARK

Chiseled square cut, center front face of inlet East side of Greenwich Road,
342 feet +/- South of the intersection of East 27th Street and Greenwich Road
Project Elevation = 1377.42
KDOT Elevation = 1378.05
(Project Elevation is -0.63 feet from KDOT)

TEMPORARY BENCHMARKS:

TBM 1:
Chiseled Square cut on E. Curb of island in center of Greenwich Road.
Approximately 60' S. of intersection of westbound ramp to K-96.
RIC Project Datum Elev. = 1373.08

TBM 2:
Chiseled Square cut on south curb of 27th Street approximately 690 feet east
of intersection of Greenwich Road.
RIC Project Datum Elev. = 1378.40

TBM 3:
Chiseled Square cut on south side of 27th Street approximately 330 feet east
of intersection of Greenwich Road.
RIC Project Datum Elev. = 1378.36

Sanitary Sewer Plans to Serve WICHITA DESTINATION DEVELOPMENT - PHASE 3 LOT 1, WICHITA DESTINATION DEVELOPMENT

an Addition to Wichita, Sedgwick County, Kansas
Gary Janzen, P.E., City Engineer
2236 PPS (607861)

OWNER / DEVELOPER

Wichita Destination Developers, Inc.
Attn: Michael J. Boyd, President
1707 North Waterfront Parkway
Wichita, Kansas 67206

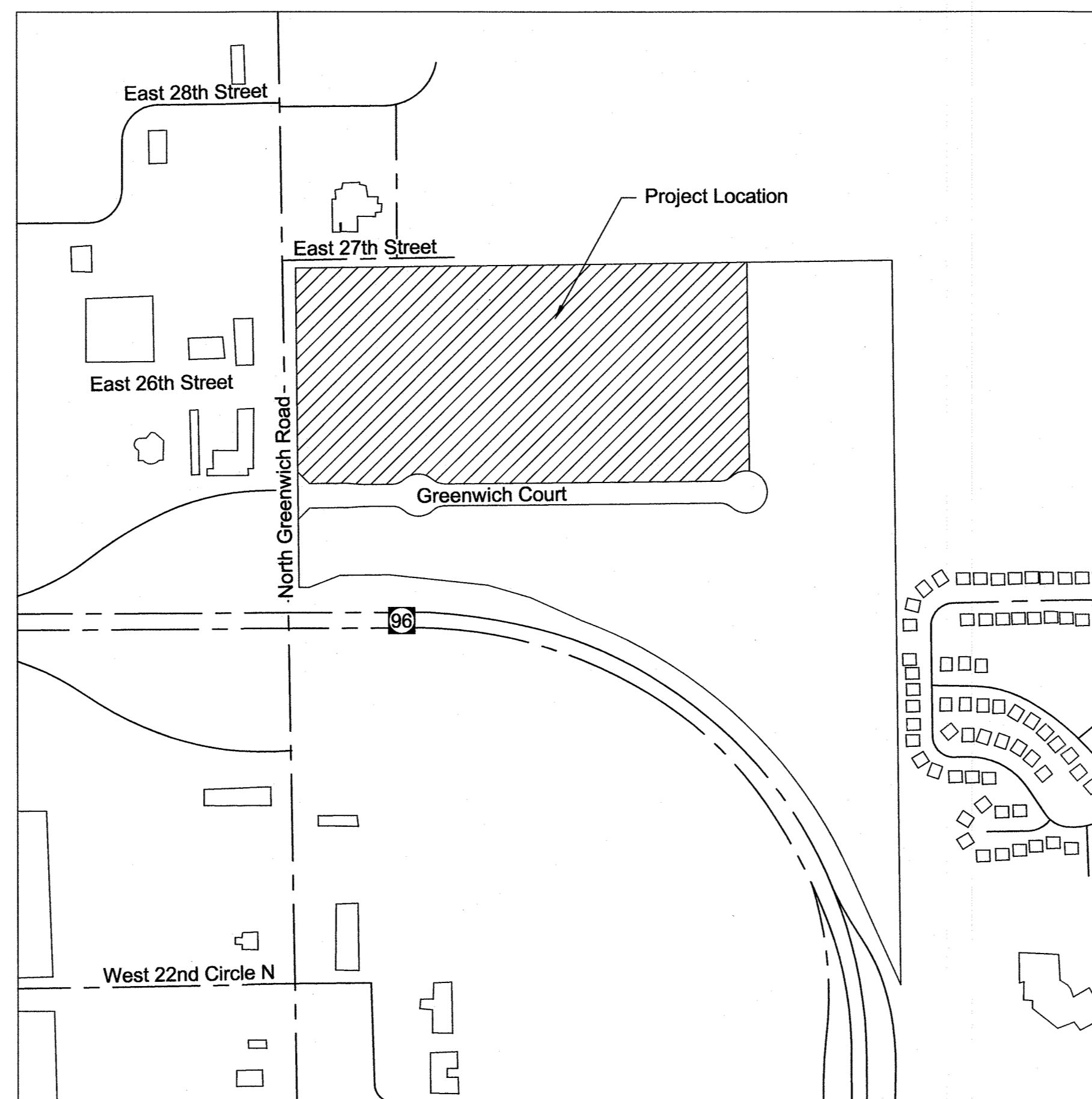
CONSULTANT / APPLICANT

Renaissance Infrastructure Consulting
1138 Cambridge Circle Drive
Kansas City, Kansas 66103

INDEX OF SHEETS

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T. Mason - City of Wichita, Inspector
Stub & Risers
Release Date: 10/19/2015
:APRosas 10/20/2015



LOCATION MAP
Scale: 1" = 500'

LEGEND

- | | |
|----------------------------------|------------------------------|
| — Existing Section Line | — Proposed Right-of-Way |
| - - - Existing Right-of-Way Line | — Proposed Property Line |
| — Existing Lot Line | — Proposed Lot Line |
| - - - Existing Easement Line | — Proposed Easement |
| — Existing Curb & Gutter | — Proposed Curb & Gutter |
| — Existing Sidewalk | — Proposed Sidewalk |
| — Existing Storm Sewer | — Proposed Storm Sewer |
| □ Existing Storm Structure | □ Proposed Storm Structure |
| - - - Existing Waterline | A Proposed Fire Hydrant |
| - - - Existing Gas Main | — Proposed Waterline |
| — Existing Sanitary Sewer | — Proposed Sanitary Sewer |
| ● Existing Sanitary Manhole | ● Proposed Sanitary Manhole |
| - - - Existing Contour Major | — Proposed Contour Major |
| - - - Existing Contour Minor | — Proposed Contour Minor |
| | ----- Future Curb and Gutter |

AS-BUILT PLANS
CONTRACTOR: MIES CONSTRUCTION
SUPERINTENDENT: ALBERT WILLIAMS
FOREMAN: ALBERT WILLIAMS
CLIENT: CITY OF WICHITA
INSPECTOR: JACOB MORRIS, SCHWAB-EATON, P.A.
PDF BY: JCM 10/13/15

APPROVED AS NOTED
BY WICHITA PUBLIC WORKS
ENGINEERING DIVISION

Engineering: *Rebecca Dief* 7-29-15
Utilities: *Jacob Morris* 7-29-15

NOTE TO CONTRACTORS

Inspection and testing for this project is to be provided by a Licensed Consulting Engineering Firm under contract with the Owner / Developer. Said Inspection to be in accordance with the City of Wichita standard construction engineering practices and certified by a Licensed Professional Engineer in the State of Kansas. No work shall be performed in dedicated easements or public right-of-way by the Contractor without such inspection nor shall any work be commenced without written authorization by the City Engineer. All Construction and Materials shall comply with the City of Wichita Specifications and Standards (on file and available in the City's Engineering Office).

An Approved copy of these plans signed by City staff are required on-site.

UTILITY SERVICE & INSTALLATION CONTACTS

- | | |
|---|---|
| KANSAS GAS SERVICE
Attn: Tim Hamlin
(316) 832-3121 | WICHITA WATER
Attn: Greg Lolley
(316) 268-4334 |
| WESTAR ENERGY
Attn: Becky Thompson
(316) 261-6320 | AT&T
Attn: Jason Edwards
(316) 268-2008 |
| BLACK HILLS ENERGY
Attn: Daryl Keller
(316) 941-1654 | COX COMMUNICATIONS
Attn: Mark Henderson
(316) 260-7745 |
| NUSTAR ENERGY L.P.
Attn: Renee Davis
(316) 721-7059 | WICHITA SEWER
Attn: LaDonna
(316)268-4329 |

UTILITY EMERGENCY CONTACTS

- | | |
|---|--|
| KANSAS ONE-CALL
(316) 687-2470 | AT&T
1-555-1212 |
| COX COMMUNICATIONS
(316) 687-2470 | CITY OF WICHITA WATER DEPT
(316) 268-4908 |
| WESTAR ENERGY/
KANSAS GAS & ELECTRIC COMPANY
(800) 482-4950 | CITY OF WICHITA SEWER DEPT
(316) 268-4071 |
| AQUILA NATURAL GAS
(316) 941-1608
(800) 303-0357 | KANSAS ONE-CALL:
1-800-DIG-SAFE
(1-800-344-7235) |

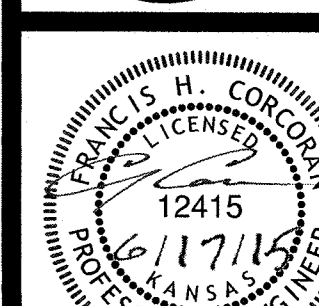


Protect yourselves and your property against underground utility damage and liability.
Find out where the underground utility lines might be buried before you dig.
Anyone digging in Kansas must call before digging. The person who is doing the work is responsible for calling KOC. If the owner contracts with a professional excavator to do the excavation then the professional excavator is responsible for calling KOC.
You (the digger) will need to provide information about the work site when you call. This is a FREE service.
CALL BEFORE YOU DIG
IT'S THE LAW.

JUNE 2015

NO.	BY	DATE	REVISION
2	JAR	FHC 06/15/15	PER CITY COMMENTS
1	JAR	RLC 06/03/15	PER CITY COMMENTS
1	JAR	FHC 09/15/15	ORIGINAL SUBMITTAL

Renaissance Infrastructure Consulting
1138 W. CAMBRIDGE CIRCLE DRIVE
KANSAS CITY, KANSAS 66103
WWW.RIC-CONSULT.COM



46-156-21

FINAL PLAT

WICHITA DESTINATION DEVELOPMENT

AN ADDITION TO WICHITA, SEDGWICK COUNTY, KANSAS

A REPLAT OF ALL OF K96 AND GREENWICH NORTH ADDITION

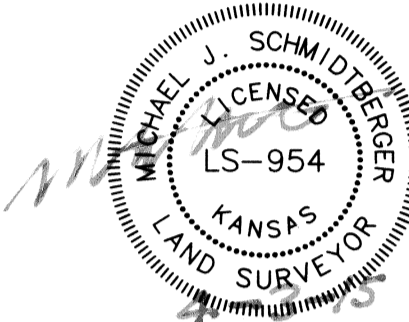
CERTIFICATE OF SURVEY

I, Michael J. Schmidtberger, a registered land surveyor in Kansas, do hereby certify that I have been in responsible charge of surveying and platting of "WICHITA DESTINATION DEVELOPMENT" an addition to Wichita, Sedgwick County, Kansas, into Lots, Reserves and Streets the same being accurately set forth in the accompanying plat and described herein:

A replat of all of Lots 1 thru 7 inclusive, Block 1, Lots 1 thru 8 inclusive, Block 2, Reserves A, B, C, D, E, F and G, K96 AND GREENWICH NORTH ADDITION, an addition to Wichita, Sedgwick County, Kansas, together with WOODSPRING ST, WOODSPRING CIR., and BOULDER DR. platted rights-of-way.

All lots, blocks, streets, easements, setbacks, and access controls, together with all other public dedications or rights-of-way within the above described property are hereby vacated and replatted by virtue of K.S.A. 12-512b, as amended.

I hereby certify that the details of this plat are correct to the best of my knowledge and belief this 5th day of April, 2015.



Michael Schmidtberger, Kansas LS-954
Renaissance Infrastructure Consulting
1138 W. Cambridge Circle Drive
Kansas City, Kansas 66103

OWNER'S CERTIFICATE

Know all men by these presents that we the undersigned property owner of the land above set forth in the Registered Land Surveyor's Certificate, has caused the same to be surveyed and platted into Lots, Blocks, Reserves, and Streets the same to be known as "WICHITA DESTINATION DEVELOPMENT," a replat K96 AND GREENWICH NORTH ADDITION, an addition to Wichita, Sedgwick County, Kansas.

This plat shall conform to the recitals of CUP DP-333.

Easements for the construction and maintenance of public utilities, drainage and sanitary sewer, as indicated on the accompanying plat are hereby granted to the public.

All streets are hereby dedicated to and for the use of the public.

A drainage plan has been developed for this plat. All drainage easements, rights-of-way, or reserves shall remain at established grades or as modified with the approval of the applicable City or County Engineer, and unobstructed to allow for the conveyance of stormwater.

All abutters rights of access to or from Greenwich Road, over and across the west line of "WICHITA DESTINATION DEVELOPMENT", are hereby granted to the appropriate governing body as indicated hereon. All abutters rights of access to or from Kansas Highway 96, over and across the south line of "WICHITA DESTINATION DEVELOPMENT", are hereby granted to the appropriate governing body as indicated hereon. Access controls are dedicated to and for the use of the public.

Lot 1, Block A, is required to adhere to the minimum pad elevation as shown on the "Minimum Pad Elevations" table.

Reserves "A", "B", "C", "D", "E", "F", and "G" are platted for monuments, signs, landscaping, berming, sidewalks, irrigation, open space, pavement, utilities confined by easements, and walls provided that they do not inhibit the conveyance of surface drainage. Reserves "A", "B", "C" are also platted for drainage, drives, and parking. Reserves "D", "E", "F" and "G" shall allow for public access across said reserves, at various locations for driveways, as approved by the City Engineer. The Reserves shall be owned and maintained by the Lot owner's association, provided however, that the undersigned or Lot owner's association as the undersigned successors in interest may, at its discretion deed parcels of said Reserves "A", "B", and "C" to an owner (s) of an adjoining lot subject to the obligation to maintain such deeded parcel in compliance with the provisions hereof and in compliance with the maintenance covenants of any applicable restrictive covenants or regulations.

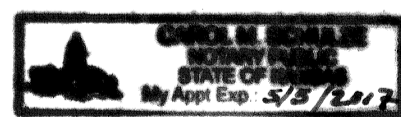
Medical Practice Association Properties, LLC, a Kansas Limited Liability Company

Aaron Ryan
Aaron Ryan, Manager

STATE OF KANSAS)
) SS
COUNTY OF SEDGWICK)

This instrument was acknowledged before me on 9th day of April, 2015, by Aaron Ryan, Manager, Medical Practice Association Properties, LLC, a Kansas Limited Liability Company.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal; the day and year last above written.



Carol M. Schulze, Notary Public
Notary Public: Carol M. Schulze
My Term Expires: May 5, 2017

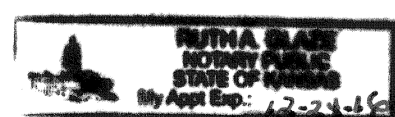
Wichita Destination Developers, Inc., a Kansas Corporation

Michael J. Boyd
Michael J. Boyd, President

STATE OF KANSAS)
) SS
COUNTY OF SEDGWICK)

This instrument was acknowledged before me on 8th day of APRIL, 2015, by Michael J. Boyd, President, Wichita Destination Developers, Inc., a Kansas Corporation.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal; the day and year last above written.



Ruth A. Glaze, Notary Public
Notary Public: Ruth A. Glaze
My Term Expires: 12-24-16

WDDMBB, LLC, a Kansas limited liability company

Michael J. Boyd
Michael J. Boyd, President

STATE OF KANSAS)
) SS
COUNTY OF SEDGWICK)

This instrument was acknowledged before me on 8th day of April, 2015, by Michael J. Boyd, President, WDDMBB, LLC, a Kansas limited liability company.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal, the day and year last above written.



Ruth A. Glaze, Notary Public
Notary Public: Ruth A. Glaze
My Term Expires: 12-24-16

PLANNING COMMISSION CERTIFICATE

STATE OF KANSAS)
) SS
CITY OF WICHITA)

This plat of "WICHITA DESTINATION DEVELOPMENT" has been submitted to and approved by the Wichita-Sedgwick County Metropolitan Area Planning Commission, Wichita, Kansas.

Dated this 24 day of July, 2014

WICHITA-SEDGWICK COUNTY METROPOLITAN AREA PLANNING COMMISSION

Don Klausmeyer, Chairman
John L. Schlegel, Secretary



GOVERNING BODY CERTIFICATE

STATE OF KANSAS)
) SS
CITY OF WICHITA)

The dedications shown on this plat are hereby accepted and this plat is hereby approved by the governing body of the City of Wichita, Kansas.

Dated this 12 day of May, 2015

At the direction of the City Council;

Jeff Longwell, Mayor
Karen Sublett, City Clerk

TRANSFER RECORD

Entered on transfer record this 5th day of June, 2015

Kelly B. Arnold
Kelly B. Arnold, County Clerk



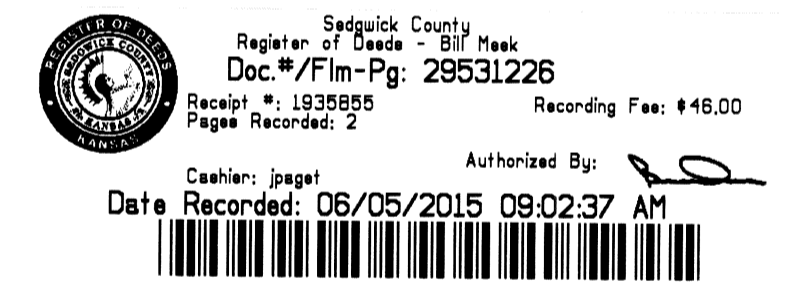
REGISTER OF DEEDS CERTIFICATE

STATE OF KANSAS)
) SS
COUNTY OF SEDGWICK)

This is to certify that this instrument was filed for record in the Register of Deeds office this 5th day of June, 2015, at 9:02:37 o'clock A.M. and is duly recorded.

Bill Meek, Register of Deeds

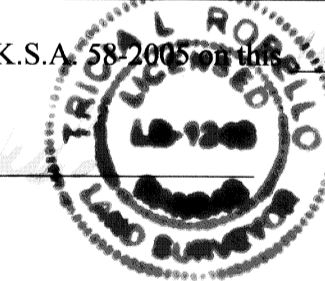
Tonya E. Buckingham, Deputy



COUNTY SURVEYOR

Reviewed in accordance with K.S.A. 12-512b on the _____ day of _____, 2014.

Tricia L. Robello, LS #1246
Deputy County Surveyor
Sedgwick County, Kansas



WICHITA DESTINATION DEVELOPMENT	
Prepared For: Wichita Destination Developers, Inc. 1707 N. Waterfront Parkway Wichita, KS 67206 (316) 685-5341	Renaissance Infrastructure Consulting
Date of Preparation: May 28, 2014	1138 W. CAMBRIDGE CIRCLE DRIVE KANSAS CITY, KANSAS 66103 913.317.9500 WWW.RIC-CONSULT.COM

FINAL PLAT

WICHITA DESTINATION DEVELOPMENT

AN ADDITION TO WICHITA, SEDGWICK COUNTY, KANSAS

A REPLAT OF ALL OF K96 AND GREENWICH NORTH ADDITION

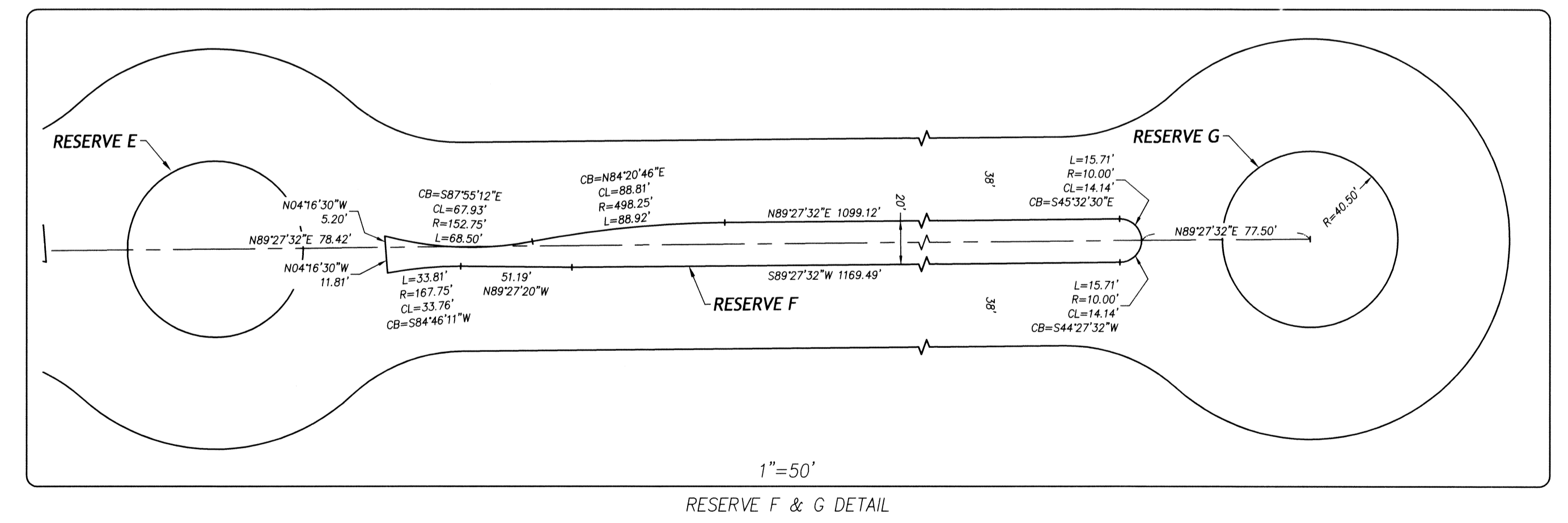
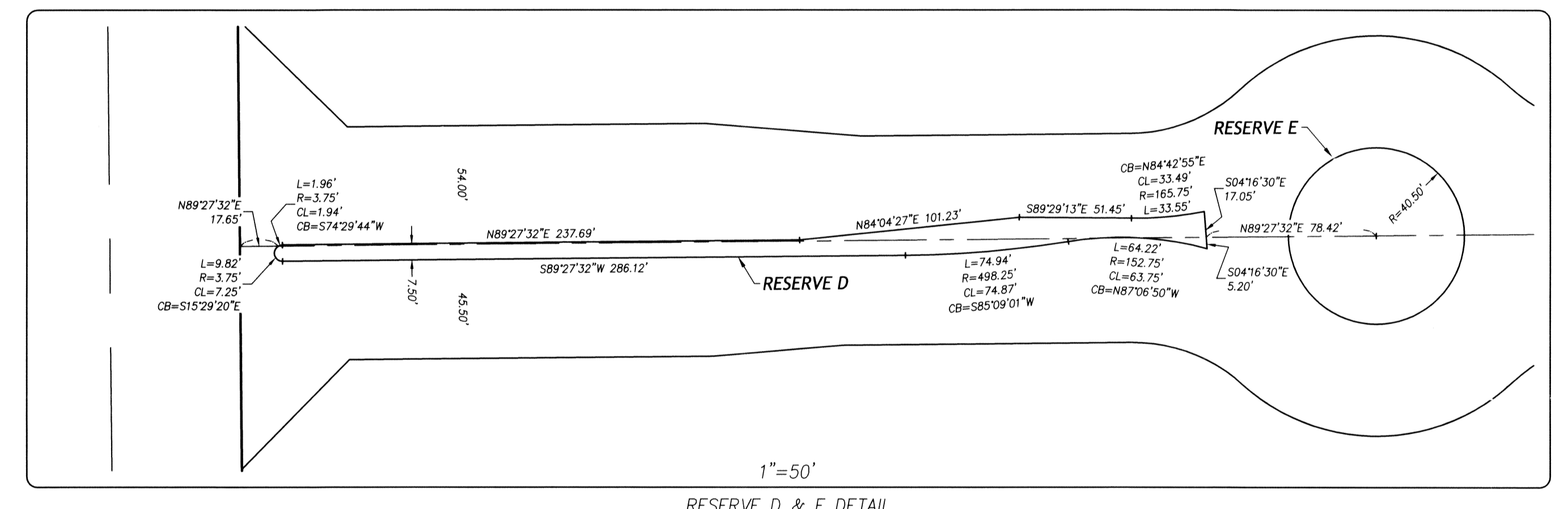
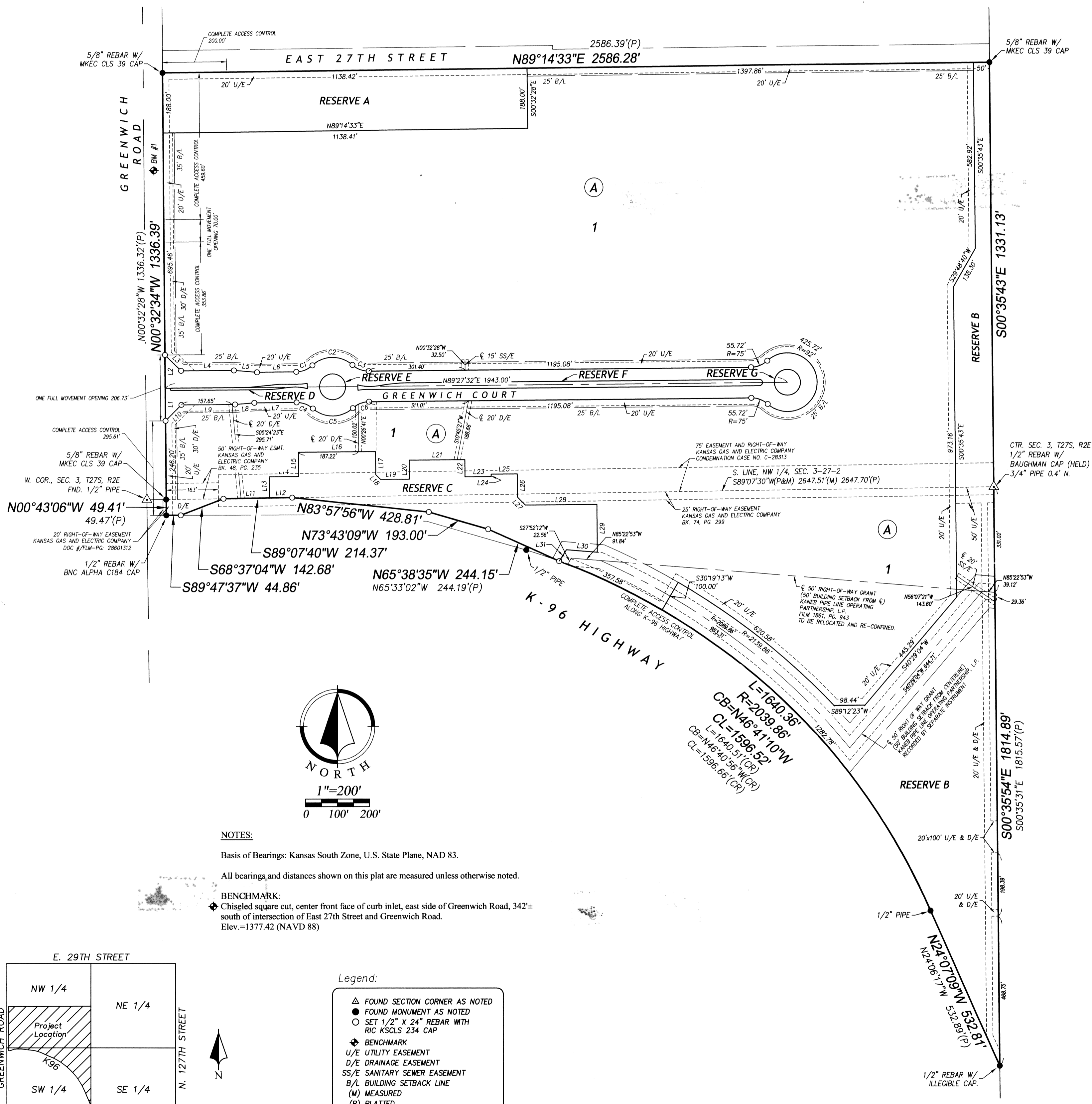
Minimum Pad Elevations	
Lot No.	Elevation
1	1374.50

Line Table		
Line #	Direction	Length
L1	N00°32'34"W	102.36'
L2	N00°32'34"W	104.37'
L3	S45°32'28"E	70.52'
L4	N89°27'32"E	164.25'
L5	S85°22'10"E	72.11'
L6	N89°27'32"E	123.11'
L7	N89°27'32"E	132.38'
L8	N85°09'27"E	60.00'
L9	N89°27'32"E	166.97'
L10	N44°27'32"E	70.51'
L11	S89°07'40"W	143.00'
L12	N89°07'40"W	71.37'
L13	S00°52'20"E	66.14'
L14	N89°27'32"E	92.50'
L15	N00°32'28"W	75.50'
L16	N89°27'32"E	240.41'

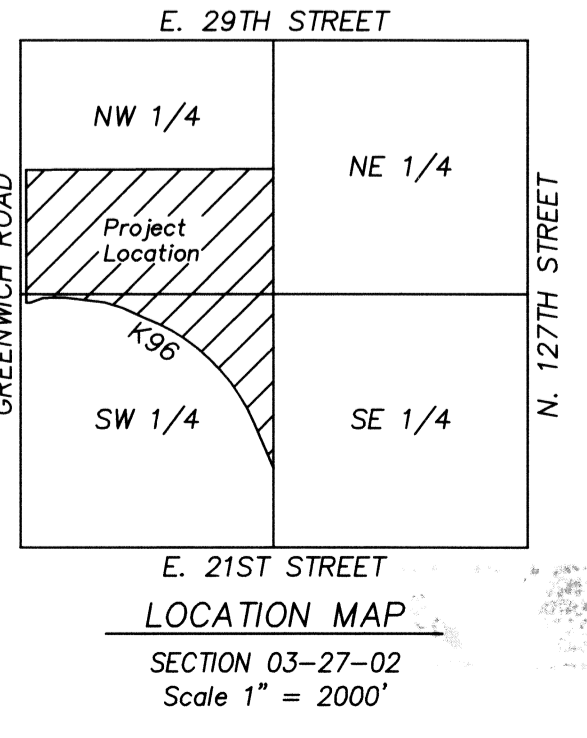
Line Table		
Line #	Direction	Length
L17	S00°32'28"E	72.50'
L18	S45°32'28"E	21.21'
L19	N89°27'32"E	90.00'
L20	N00°32'28"W	59.50'
L21	N89°27'32"E	174.00'
L22	S00°32'28"E	53.50'
L23	N89°27'32"E	81.26'
L24	N00°32'28"W	7.50'
L25	N89°27'32"E	81.24'
L26	S00°32'28"E	72.50'
L27	S45°32'28"E	35.36'
L28	N89°27'32"E	223.00'
L29	S00°32'28"E	150.59'
L30	N85°22'53"W	95.75'
L31	S32°31'45"W	42.36'

Curve Table			
Curve #	Length	Radius	Chord
C1	55.716	75.000	N68°10'38"E 54.44'
C2	136.689	92.000	S89°27'32"W 124.46'
C3	55.716	75.000	S69°15'34"E 54.44'
C4	55.716	75.000	N69°15'34"W 54.44'
C5	136.689	92.000	N89°27'32"E 124.46'
C6	55.716	75.000	S68°10'38"W 54.44'

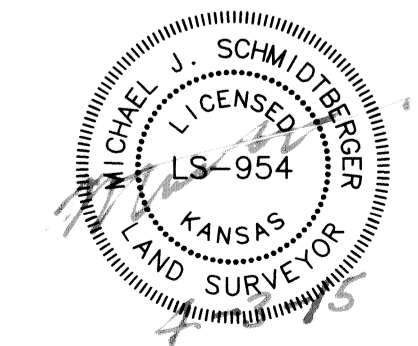
Lot Areas		
Lot #	Sq. Ft.	Ac.
1	3,364,626	77.241
RESERVE A	214,022	4.913
RESERVE B	612,416	14.059
RESERVE C	149,150	3.424
RESERVE D	3,944	0.091
RESERVE E	5,153	0.118
RESERVE F	24,390	0.560
RESERVE G	5,153	0.118



NOTES:
 Basis of Bearings: Kansas South Zone, U.S. State Plane, NAD 83.
 All bearings and distances shown on this plat are measured unless otherwise noted.
BENCHMARK:
 Chiseled square cut, center front face of curb inlet, east side of Greenwich Road, 342± south of intersection of East 27th Street and Greenwich Road.
 Elev.=1377.42 (NAVD 88)

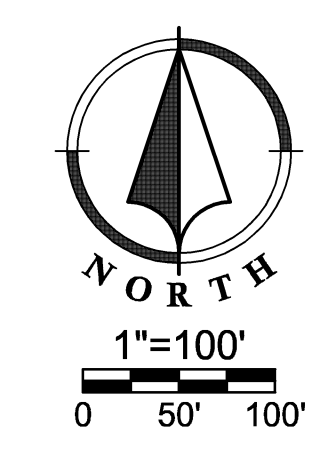
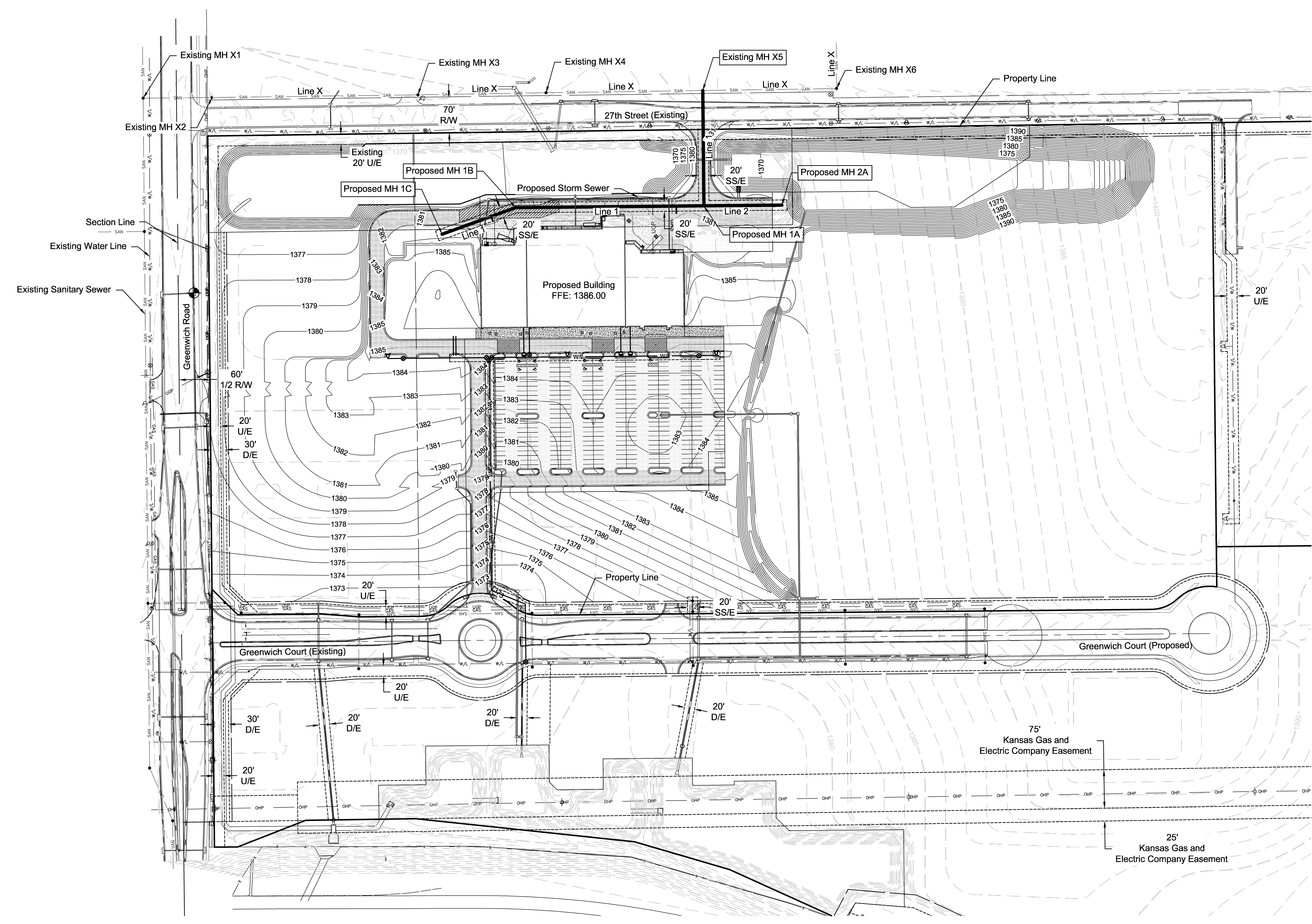


- Legend:**
- ▲ FOUND SECTION CORNER AS NOTED
 - FOUND MONUMENT AS NOTED
 - SET 1/2" X 24" REBAR WITH RIC KSCLS 234 CAP
 - ◆ BENCHMARK
 - U/E UTILITY EASEMENT
 - D/E DRAINAGE EASEMENT
 - SS/E SANITARY SEWER EASEMENT
 - B/L BUILDING SETBACK LINE
 - (M) MEASURED
 - (P) PLATED
 - L LENGTH OF CURVE
 - R RADIUS OF CURVE
 - CL CHORD LENGTH
 - CB CHORD BEARING



WICHITA DESTINATION DEVELOPMENT	
Prepared For: Wichita Destination Developers, Inc. 1707 N. Waterfront Parkway Wichita, KS 67206 (316) 685-5341	
Date of Preparation: May 28, 2014	
	Renaissance Infrastructure Consulting 1138 W. Cambridge Circle Drive Kansas City, Kansas 66103 913.317.9500 www.RIC-CONSULT.COM

GENERAL LAYOUT



K-96 Highway

75'
Kansas Gas and
Electric Company Easement

25'
Kansas Gas and
Electric Company Easement

Proposed Building
FFE: 1386.00

Existing MH X1, Existing MH X2, Existing MH X3, Existing MH X4, Existing MH X5, Existing MH X6

Proposed MH 1A, Proposed MH 1B, Proposed MH 1C, Proposed MH 2A

Proposed Storm Sewer, 20' SS/E, 20' SS/E, 20' SS/E

Line X, Line X, Line X, Line X, Line X

Line 1, Line 2

Greenwich Road, Greenwich Court (Existing), Greenwich Court (Proposed)

27th Street (Existing)

Property Line

Existing Water Line, Existing Sanitary Sewer

60' 1/2 R/W, 20' U/E, 30' D/E, 20' U/E, 30' D/E, 20' U/E, 20' D/E, 20' D/E, 20' D/E

1377, 1378, 1379, 1380, 1381, 1382, 1383, 1384, 1385, 1386, 1387, 1388, 1389, 1390, 1391, 1392, 1393, 1394, 1395, 1396, 1397, 1398, 1399, 1400

NO.	BY	DATE	REVISION
6.	JAR	RLC 07/30/15	ISSUED FOR CONSTRUCTION
5.	JAR	RLC 07/16/15	PER REVISED CASING PIPE
4.	JAR	RLC 07/16/15	PER CITY COMMENTS
3.	JAR	FHC 07/08/15	PER CITY COMMENTS
2.	JAR	FHC 06/18/15	PER CITY COMMENTS
1.	JAR	FHC 06/03/15	PER CITY COMMENTS
	JAR	FHC 05/15/15	ORIGINAL SUBMITTAL

Renaissance Infrastructure Consulting

1138 W. CAMBRIDGE CIRCLE DRIVE
KANSAS CITY, KANSAS 66103

913.317.9500
WWW.RIC-CONSULT.COM

BUILT TO PLAN

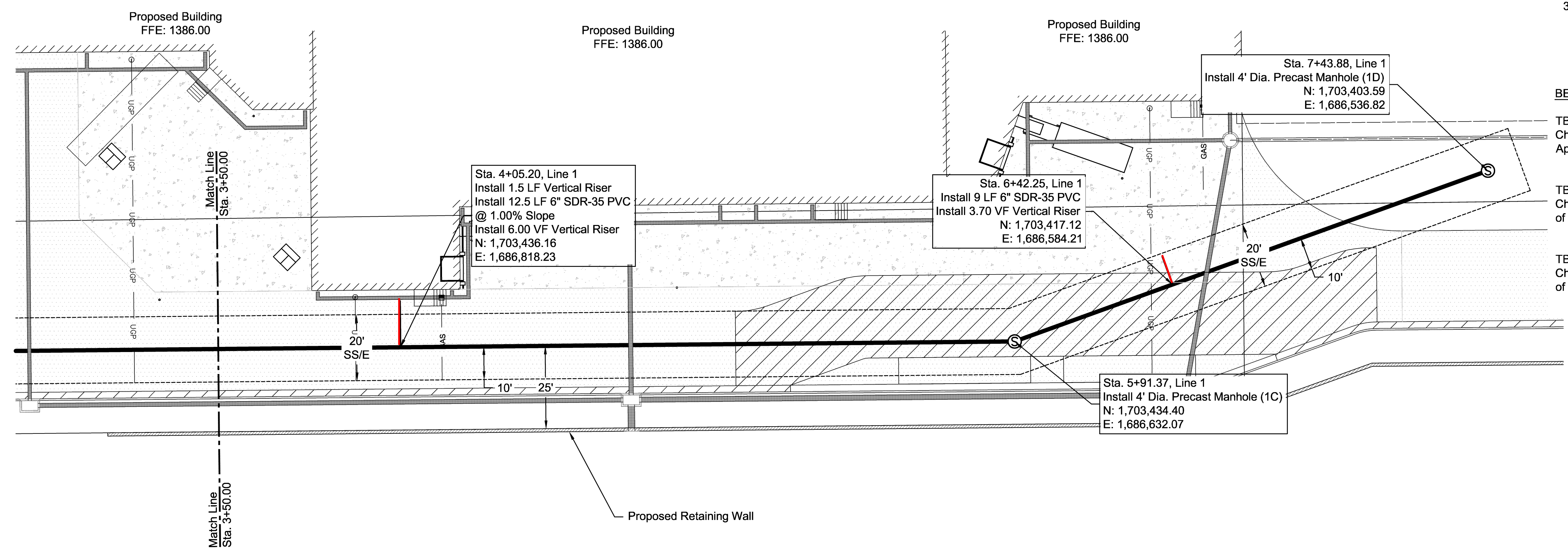
Jul 30, 2015 5:10pm
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PROJECT BENCHMARK

Chiseled square cut, center front face of inlet East side of Greenwich Road, 342 feet +/- South of the intersection of East 27th Street and Greenwich Road
Project Elevation = 1377.42
Project Elevation = 1377.42
Project Elevation = 1378.05
(Project Elevation is -0.63 feet from KDOT)

BENCHMARKS:

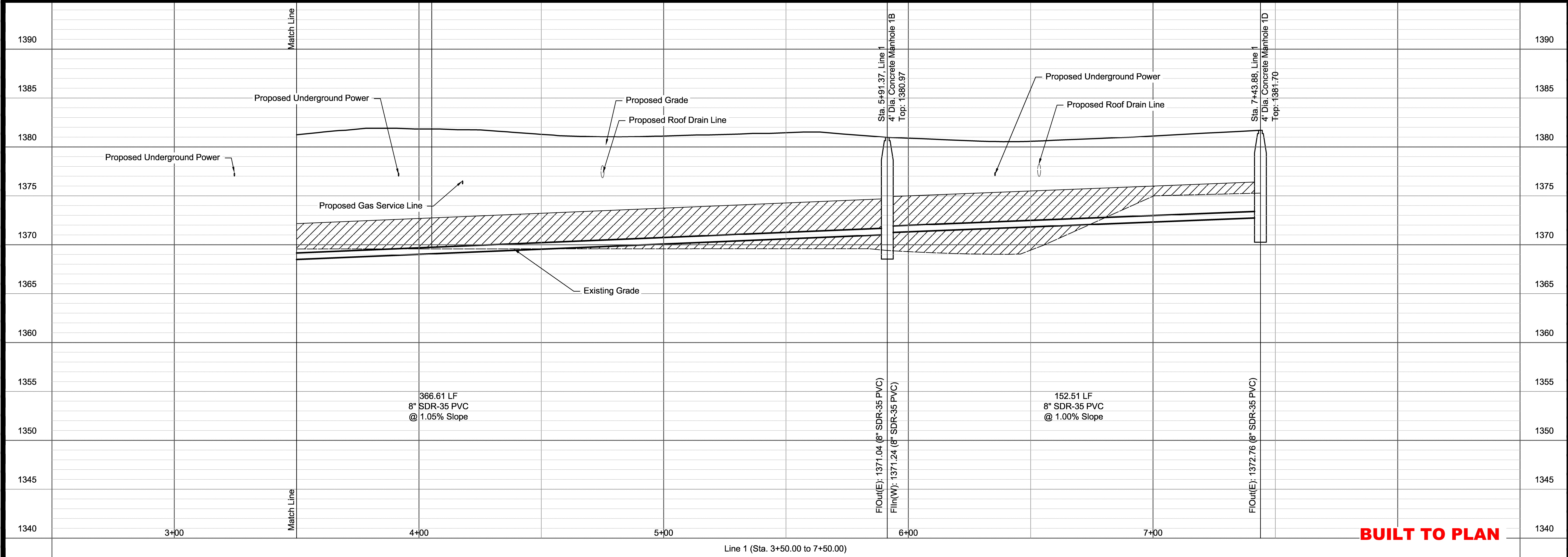
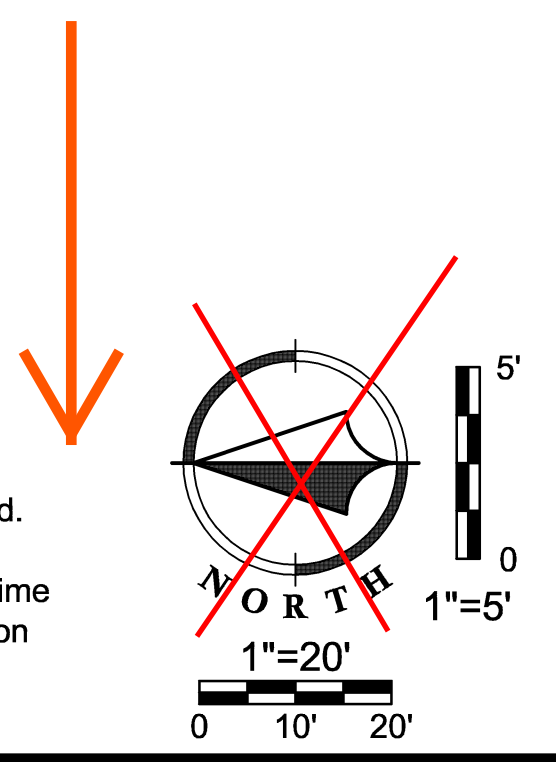
- TBM 1:
Chiseled Square cut on E. Curb of island in center of Greenwich Road. Approximately 60' S. of intersection of westbound ramp to K-96.
RIC Project Datum Elev. = 1373.08
- TBM 2:
Chiseled Square cut on south curb of 27th Street approximately 690 feet east of intersection of Greenwich Road.
RIC Project Datum Elev. = 1378.40
- TBM 3:
Chiseled Square cut on south side of 27th Street approximately 330 feet east of intersection of Greenwich Road.
RIC Project Datum Elev. = 1378.36



SANITARY SEWER SERVICE TABLE							
Station	Flowline @ Main	Vertical Riser	Flowline @ 45° Bend	Horizontal Pipe LF	Flowline @ 90° Bend	Vertical Riser VF	Flowline @ Plug
4+05.20	1369.06	1.5'	1371.70	12.5	1371.83	6.0'	1377.83
6+42.24	1371.75	0	1372.89	9	1372.98	3.7'	1376.68

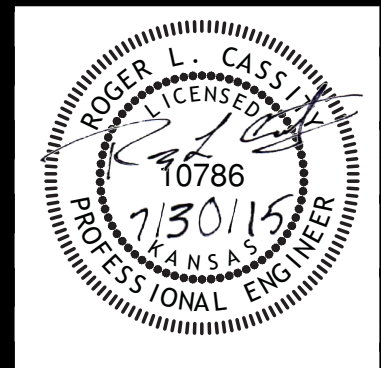
NOTE

- Existing Gas and Water Mains shall not be Disturbed.
- Location and Elevation of all Utilities is Approximate According to the Best Information Available at the Time of Design. Contractor to Verify Location and Elevation Prior to Construction.



NO.	BY	DATE	REVISION
6.	JAR	07/30/15	ISSUED FOR CONSTRUCTION
5.	JAR	07/16/15	PER REVISED CASING PIPE
4.	JAR	07/16/15	PER CITY COMMENTS
3.	JAR	07/08/15	PER CITY COMMENTS
2.	JAR	06/25/15	PER CITY COMMENTS
1.	JAR	06/03/15	ORIGINAL SUBMITTAL

Renaissance Infrastructure Consulting
913.317.9500
1138 W. CAMBRIDGE CIRCLE DRIVE
KANSAS CITY, KANSAS 66103
WWW.RIC-CONSULT.COM



BUILT TO PLAN

Line 1 (Sta. 3+50.00 to 7+50.00)

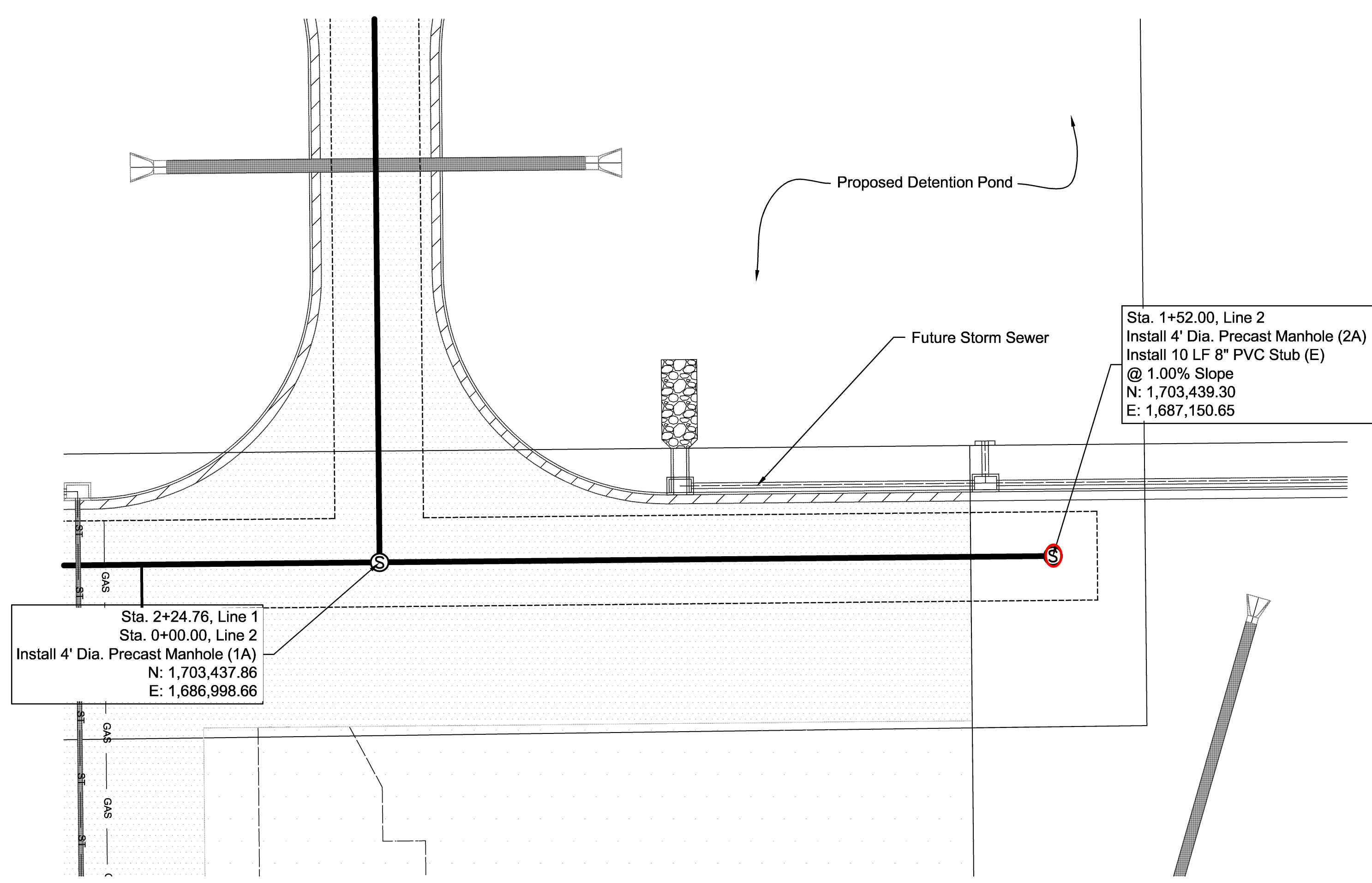
July 30, 2015 5:10pm
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PROJECT BENCHMARK

Chiseled square cut, center front face of inlet East side of Greenwich Road,
342 feet +/- South of the intersection of East 27th Street and Greenwich Road
Project Elevation = 1377.42
KDOT Elevation = 1378.05
(Project Elevation is -0.63 feet from KDOT)

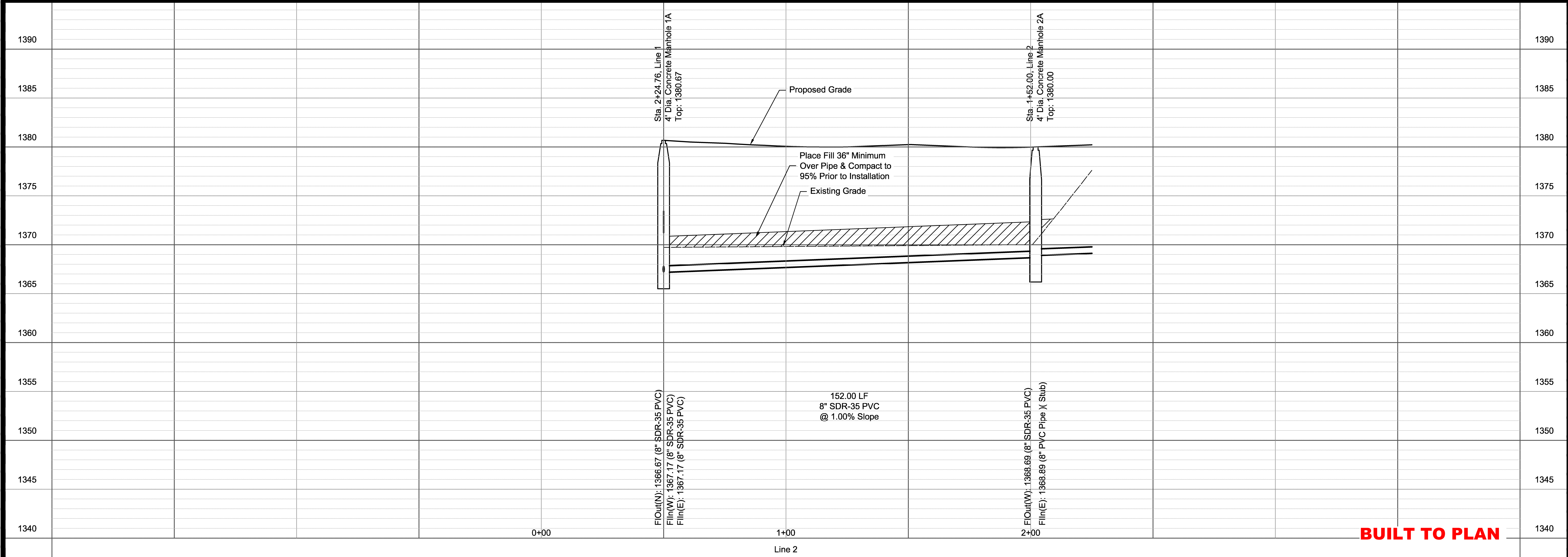
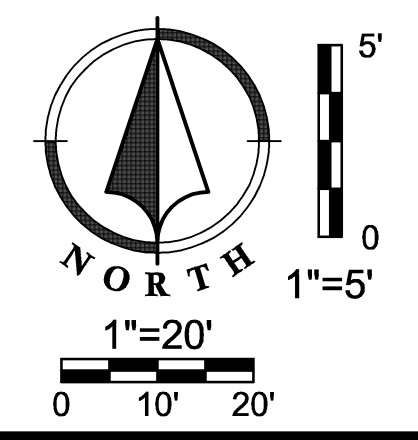
BENCHMARKS:

- TBM 1:**
Chiseled Square cut on E. Curb of island in center of Greenwich Road.
Approximately 60' S. of intersection of westbound ramp to K-96.
RIC Project Datum Elev. = 1373.08
- TBM 2:**
Chiseled Square cut on south curb of 27th Street approximately 690 feet east
of intersection of Greenwich Road.
RIC Project Datum Elev. = 1378.40
- TBM 3:**
Chiseled Square cut on south side of 27th Street approximately 330 feet east
of intersection of Greenwich Road.
RIC Project Datum Elev. = 1378.36



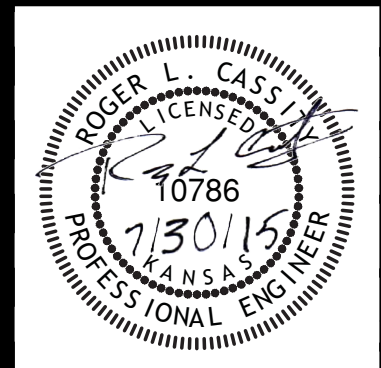
NOTE

- Existing Gas and Water Mains shall not be Disturbed.
- Location and Elevation of all Utilities is Approximate According to the Best Information Available at the Time of Design. Contractor to Verify Location and Elevation Prior to Construction.



ISSUED FOR CONSTRUCTION	
6.	JAR RLC 07/30/15
5.	JAR RLC 07/16/15
4.	JAR RLC 07/16/15
3.	JAR RLC 07/08/15
2.	JAR RLC 06/18/15
1.	JAR RLC 06/03/15
NO.	BY DATE

Renaissance Infrastructure Consulting
913.317.9500
1138 W. CAMBRIDGE CIRCLE DRIVE
KANSAS CITY, KANSAS 66103
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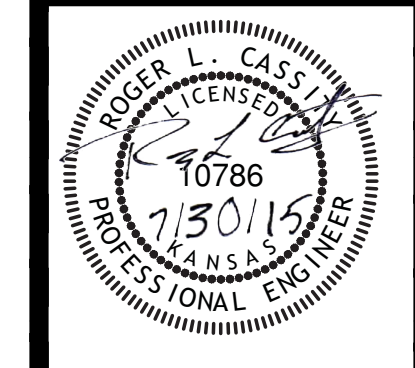


BUILT TO PLAN

Jul 30, 2015 5:10pm
 C:\Users\ADAW1\OneDrive\Work\Projects\2015\15-07-15\15-07-15-FPS.dwg

NO.	BY	DATE	REVISION
6.	JAR	07/30/15	ISSUED FOR CONSTRUCTION
5.	JAR	07/16/15	PER REVISED CASING PIPE
4.	JAR	07/16/15	PER CITY COMMENTS
3.	JAR	07/08/15	PER CITY COMMENTS
2.	JAR	06/18/15	PER CITY COMMENTS
1.	JAR	06/03/15	PER CITY COMMENTS
	JAR	05/15/15	ORIGINAL SUBMITTAL

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 KANSAS CITY, KANSAS 66103
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Disturbed Area for Site Improvements : 1.05 Acres

ESTIMATED EARTHWORK

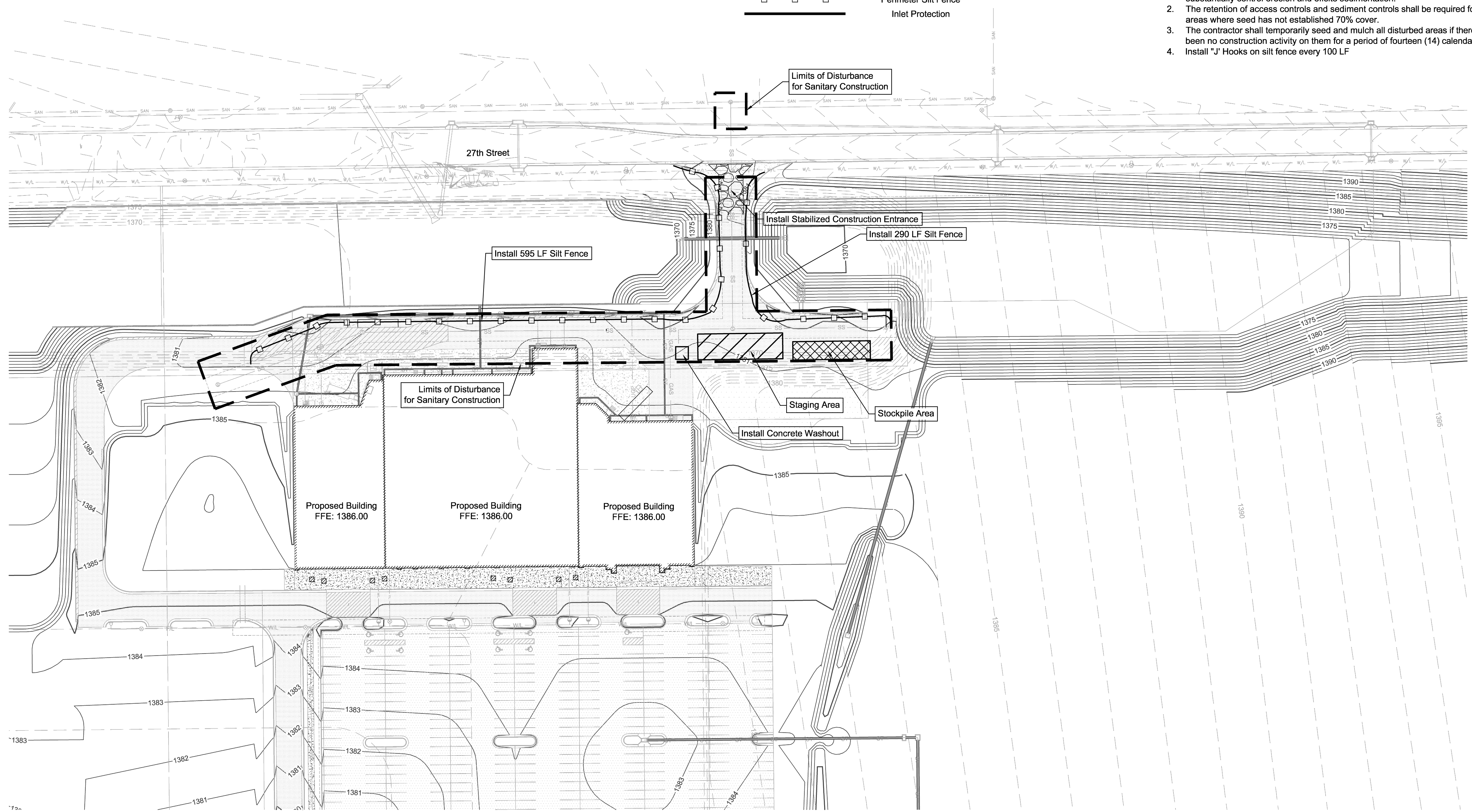
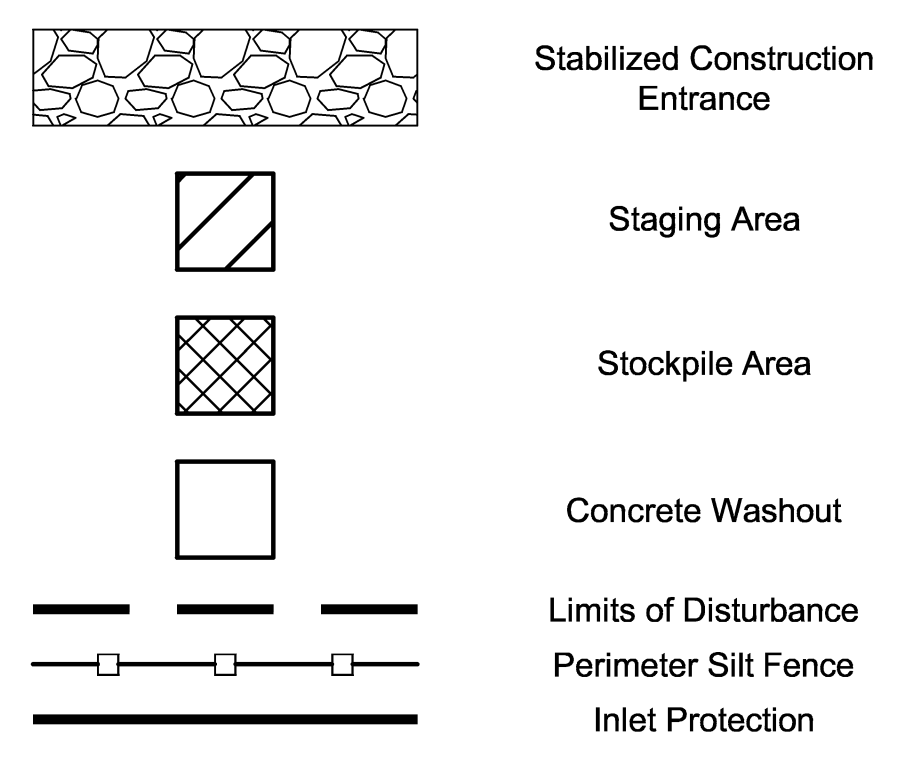
Cut: 176,797 CY
Fill: 141,463 CY

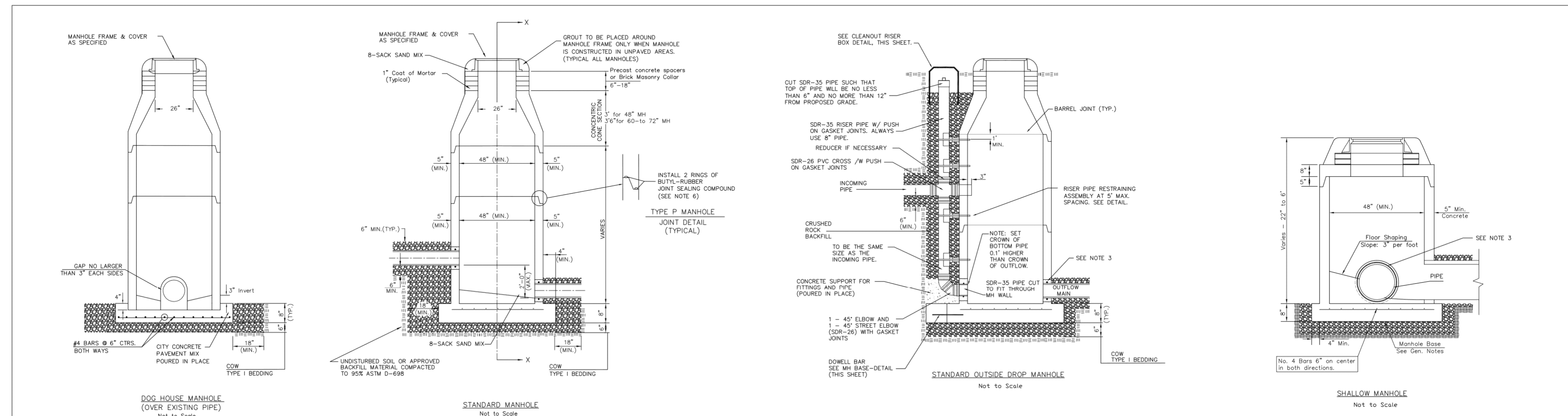
Earthwork calculations are informational only. Contractor shall be responsible for their own earthwork calculations and perform all necessary earthwork shown herein without additional cost to the owner if quantities differ than above. Earthwork numbers are unadjusted. Earthwork is for Entire Site Mass Grading.

EROSION CONTROL NOTES

1. Erosion control plan modifications shall be required if the plan fails to substantially control erosion and offsite sedimentation.
2. The retention of access controls and sediment controls shall be required for areas where seed has not established 70% cover.
3. The contractor shall temporarily seed and mulch all disturbed areas if there has been no construction activity on them for a period of fourteen (14) calendar days.
4. Install 'J' Hooks on silt fence every 100 LF

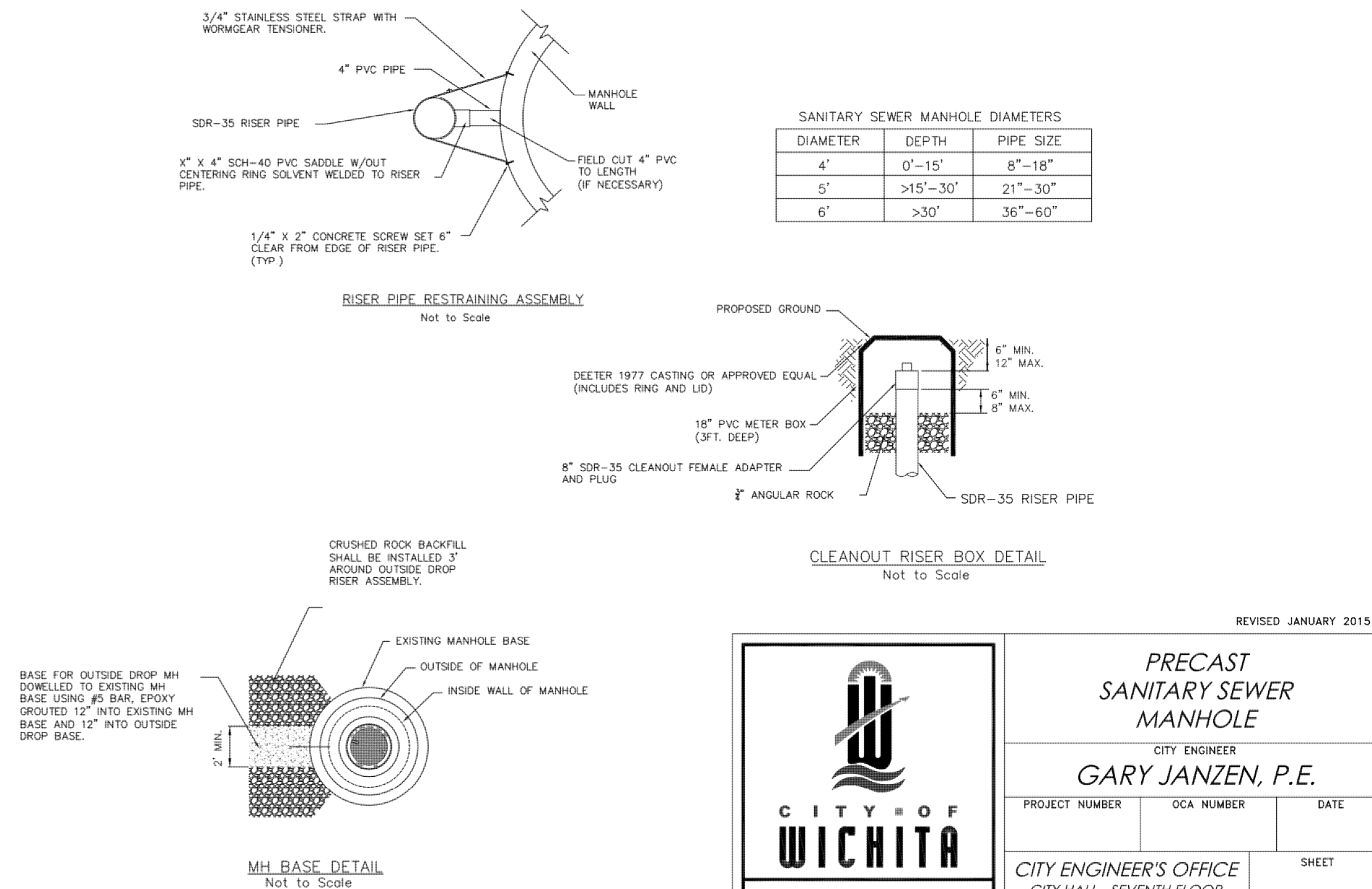
EROSION CONTROL LEGEND





PRECAST MANHOLE GENERAL NOTES

- ALL PRECAST CONCRETE MANHOLE SECTIONS SHALL CONFORM TO THE LATEST REVISIONS OF A.S.T.M. C478 AS MODIFIED BY THE SPECIFICATIONS.
- NON-SHRINK GROUT SHALL BE NON-METALLIC TYPE.
- APPROVED FLEXIBLE WATERSTOP SHALL BE INSTALLED TO JOIN THE SEWER PIPE TO THE MANHOLE WALL. THE SEWER PIPE SHALL BE SUPPORTED WITH CRUSHED ROCK A MINIMUM OF 3 FEET FROM THE MANHOLE WALL AND TO THE FIRST JOINT FOR V.C.P. SUCH THAT THE JOINT REMAINS FLEXIBLE.
- ALL INSIDE SURFACES OF THE CONCRETE MANHOLE WHICH WOULD BE EXPOSED TO SEWER GAS SHALL BE COATED PER SECTION 804.4 OF STANDARD SPECIFICATIONS.
- EXTERIOR MANHOLE WALLS SHALL BE COATED PER SECTION 804.4 OF STANDARD SPECIFICATIONS.
- JOINT SEALING COMPOUND SHALL BE PER 804.4 OF STANDARD SPECIFICATIONS.
- ALL MANHOLE SECTION JOINTS THAT WILL BE IN GROUNDWATER OR GREATER THAN 12' DEEP SHALL BE WRAPPED WITH AN EXTERNAL JOINT SEAL PER SECTION 804.4 OF STANDARD SPECIFICATIONS, AS INDICATED BY THE PLANS.
- PRECAST MANHOLES SHALL BE SET AT LEAST 4 INCHES INTO THE MANHOLE BASE FOR DOG HOUSE MANHOLES.
- TOP OF MANHOLE FLOOR SLAB SHALL BE AT LEAST 3 INCHES BELOW THE FLOW LINE OF THE OUTLET PIPE TO INSURE SUFFICIENT MINIMUM THICKNESS OF SHAPED INVERT.
- LIFTING HOLES SHALL BE FILLED WITH NON-SHRINK GROUT AND THE INTERIOR SURFACE COATED AS SPECIFIED.
- MORTAR USED IN MASONRY CONSTRUCTION SHALL CONTAIN 8 SACKS OF CEMENT PER CUBIC YARD. CONCRETE USED IN MANHOLE BASES SHALL CONFORM TO THE REQUIREMENTS OF CONCRETE FOR CONCRETE PAVEMENT CONSTRUCTION AS SPECIFIED IN THE CITY STANDARD PAVING SPECIFICATIONS USING CITY CONCRETE PAVEMENT MIX WITHOUT AIR ENTRAINING ADMIXTURE. MORTAR SHALL BE PLACED AROUND THE MANHOLE RING AS SHOWN ON THE DRAWINGS WHEN MANHOLES ARE CONSTRUCTED IN UNPAVED AREAS. COMPLETED MANHOLE SHALL BE WITHOUT LEAKS AND WATER TIGHT.
- REINFORCING STEEL SHALL BE INSTALLED IN THE MANHOLE BASES AND SHALL CONSIST OF NO.4 BARS PLACED ON 6" CENTERS IN BOTH DIRECTIONS. THE MANHOLE BASE REINFORCEMENT SHALL BE PLACED AT LEAST 3" ABOVE THE BOTTOM OF THE MANHOLE BASE. ALL COSTS FOR FURNISHING AND INSTALLING REINFORCING STEEL SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE MANHOLE.
- WALL THICKNESS SHALL BE 1" GREATER THAN MANHOLE DIAMETER IN FEET.
- OPENINGS SHALL BE CORE DRILLED INTO THE MANHOLE WALL WHEN OUTSIDE DROPS ARE CONSTRUCTED ON EXISTING MANHOLES. SUCH OPENINGS DRILLED INTO EXISTING MANHOLES SHALL BE AS SMALL AS PRACTICAL TO FACILITATE INSTALLING AND GROUTING THE NEW PIPE IN PLACE. WATERSTOP GASKETS SHALL BE USED WITH P.V.C. PIPE. THE NEW PIPE SHALL BE GROUTED INTO THE OPENING USING AN APPROVED NONSHRINK GROUT FOR THE FULL MANHOLE WALL THICKNESS. THE EXTERIOR OF THE COMPLETED CONNECTION SHALL BE SEALED WITH AN APPROVED BITUMINOUS COATING SUCH THAT THE CONNECTION WILL BE WATER TIGHT. FLOOR OF MANHOLE SHALL BE MODIFIED TO FORM NEW FLOW CHANNEL FOR THE NEW CONNECTION AS INDICATED BY THE DRAWING. THIS WORK, INCLUDING MODIFICATION OF MANHOLE FLOOR, SHALL BE PAID FOR AT THE UNIT PRICE BID FOR OUTSIDE DROP STACK CONSTRUCTED ON EXISTING MANHOLE.
- THE FLOORS OF ALL MANHOLES SHALL BE SHAPED WITH FLOW CHANNELS SUCH THAT THE MANHOLES WILL BE SELF-CLEANING AND FREE OF AREAS WHERE SOLIDS COULD BE DEPOSITED AS SEWAGE FLOWS THROUGH THE MANHOLE FROM ALL INLET PIPES TO THE OUTLET PIPE. FLOW CHANNELS SHALL BE FORMED TO MATCH THE BOTTOM HALVES OF THE INFLOWING PIPES AND THE OUTFLOWING PIPE AS SHOWN BY THE DRAWINGS. MANHOLE FLOORS SHALL HAVE SLOPES OF 3 INCHES PER FOOT IN THE AREAS OUTSIDE OF THE FLOW CHANNELS SLOPED TOWARD THE FLOW CHANNELS. PIPES LAID THROUGH MANHOLES SHALL HAVE THE TOP HALF REMOVED TO NEAT LINES FOR THE FULL INSIDE DIAMETER OF THE MANHOLE. MANHOLE FLOORS SHALL THEN BE SHAPED AROUND THE BOTTOM HALF OF THE PIPE WHICH FORMS THE FLOW CHANNEL.
- MANHOLE COVER CASTINGS AND MANHOLE FRAME CASTINGS SHALL CONFORM TO THE REQUIREMENTS AS INDICATED IN THE STANDARD SPECIFICATIONS AND AS SHOWN IN THE STANDARD DETAIL DRAWING.
- THE VERTICAL DROP IN STANDARD MANHOLES SHALL NOT EXCEED 2' REGARDLESS OF PIPE SIZE. THE CROWNS OF INFLOWING PIPES SHALL NEVER BE SET LOWER THAN THE CROWN OF THE OUTFLOWING PIPE.
- STANDARD MANHOLES SHALL BE BID AS STANDARD MANHOLES FOR THE TYPE AND DIAMETER INDICATED. OUTSIDE DROP MANHOLES SHALL BE BID AS STANDARD OUTSIDE DROP MANHOLES FOR THE TYPE AND DIAMETER INDICATED. ALL MANHOLE DIAMETERS WILL BE 4' UNLESS INDICATED OTHERWISE.
- PRECAST CONCRETE SPACERS OR BRICK MASONRY COLLAR SHALL BE INSTALLED BETWEEN THE CAST IRON FRAME AND THE CONCENTRIC CONE. THE COLLAR WILL HAVE 8" WALLS AND A VERTICAL HEIGHT OF 6" MINIMUM AND 18" MAXIMUM. A 1" COAT OF MORTAR WILL BE PLASTERED ON THE OUTSIDE OF THE COLLAR. THE USE OF PRE-CAST CONCRETE SPACERS FOR MANHOLE TOP ADJUSTMENT IS ALSO ALLOWED.
- THE FULL DIAMETER OF THE MANHOLE SHALL EXTEND THE ENTIRE DEPTH OF THE MANHOLE TO THE CONE SECTION. NO REDUCTION IN MANHOLE DIAMETER WILL BE ALLOWED.
- REFER TO PLANS FOR SIZE OF OUTSIDE DROP RISER, SADDLES AND CROSS.



REVISED JANUARY 2015

PRECAST SANITARY SEWER MANHOLE

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER OCA NUMBER DATE

CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

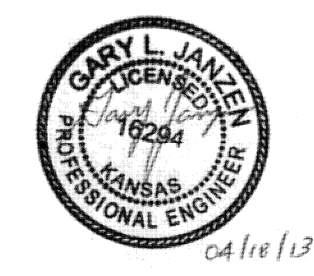
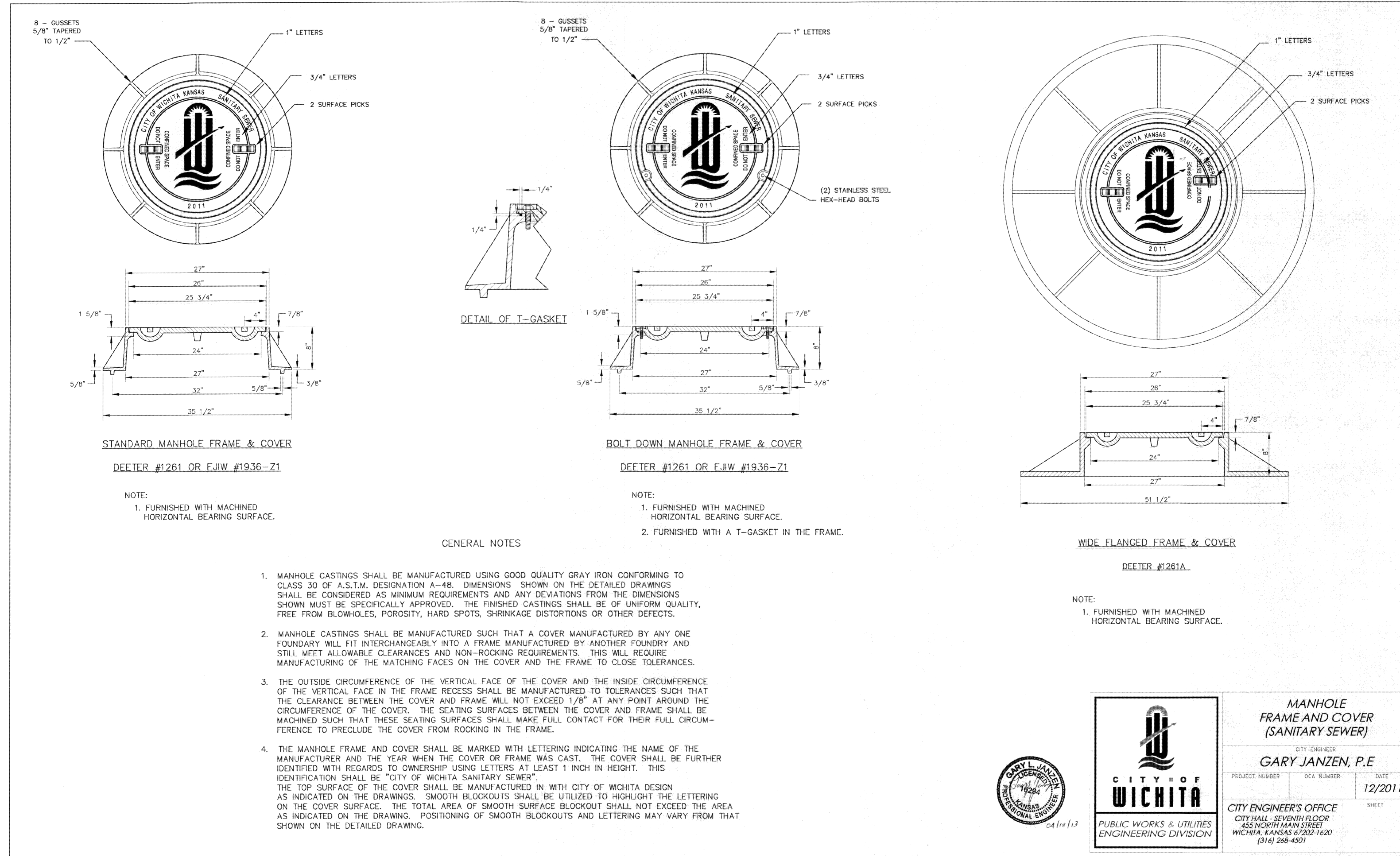
SHEET

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KANSAS CITY, KANSAS 66103

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NO.	BY	DATE	REVISION	
6.	JAR	RLC	07/30/15	ISSUED FOR CONSTRUCTION
5.	JAR	RLC	07/16/15	PER REVISED CABING PIPE
4.	JAR	RLC	07/15/15	PER CITY COMMENTS
3.	JAR	RLC	07/08/15	PER CITY COMMENTS
2.	JAR	RLC	06/15/15	PER CITY COMMENTS
1.	JAR	RLC	06/03/15	PER CITY COMMENTS
	JAR	RLC	05/15/15	ORIGINAL SUBMITTAL



CITY OF WICHITA
 PUBLIC WORKS & UTILITIES
 ENGINEERING DIVISION

MANHOLE FRAME AND COVER (SANITARY SEWER)
 CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER: _____ OCA NUMBER: _____ DATE: **12/2011**

CITY ENGINEER'S OFFICE
 CITY HALL - SEVENTH FLOOR
 455 NORTH MAIN STREET
 WICHITA, KANSAS 67202-1620
 (316) 268-4501

SHEET NO. _____ OF _____

SS-102

NO.	BY	DATE	REVISION
6.	JAR	07/30/15	ISSUED FOR CONSTRUCTION
5.	JAR	07/16/15	PER REVISED CASING PIPE
4.	JAR	07/15/15	PER CITY COMMENTS
3.	JAR	07/08/15	PER CITY COMMENTS
2.	JAR	06/18/15	PER CITY COMMENTS
1.	JAR	06/03/15	PER CITY COMMENTS
	JAR	05/15/15	ORIGINAL SUBMITTAL

NO.	BY	DATE	REVISION
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4.	JAR	07/15/15	PER CITY COMMENTS
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2.	JAR	06/18/15	PER CITY COMMENTS
1.	JAR	06/03/15	PER CITY COMMENTS
	JAR	05/15/15	ORIGINAL SUBMITTAL

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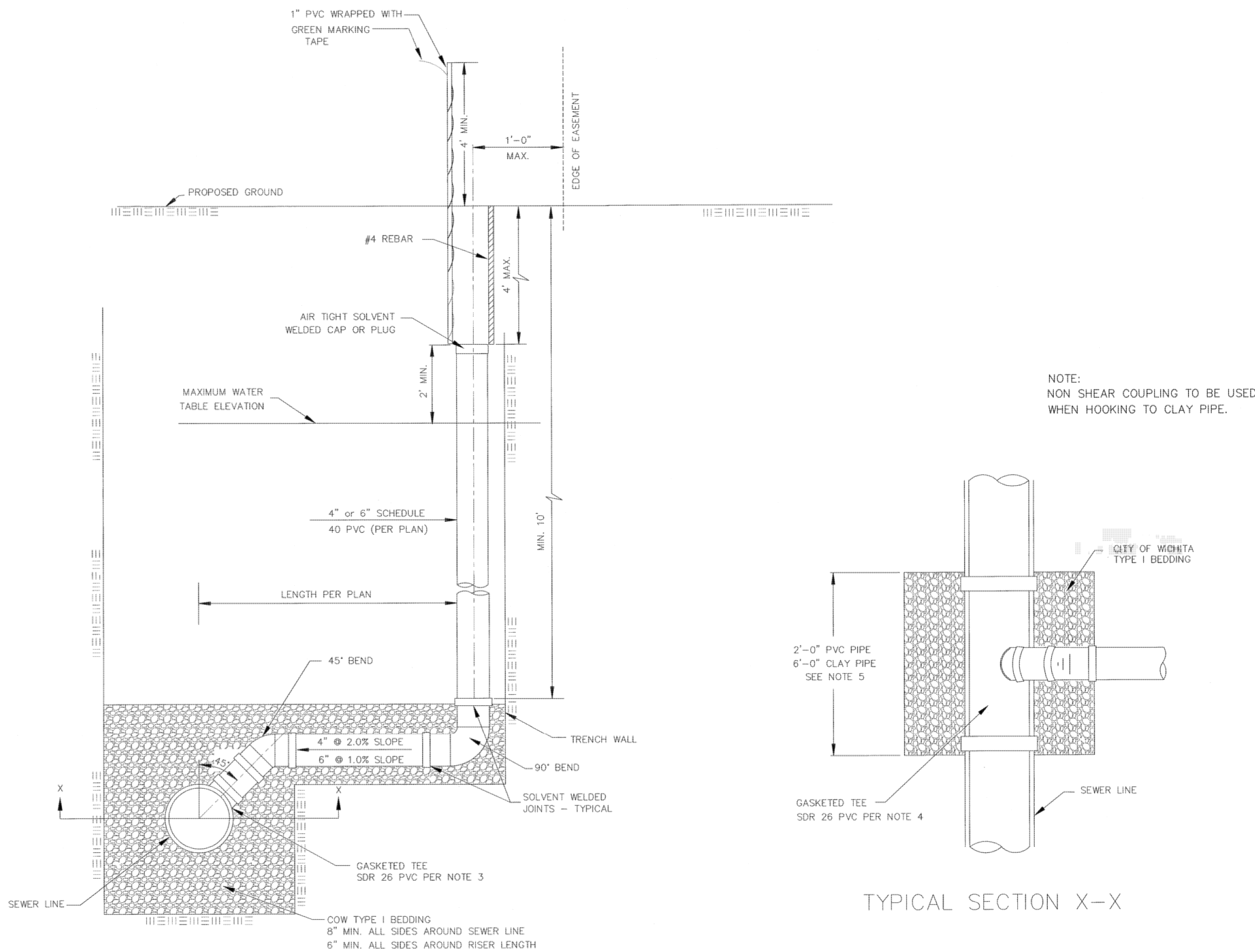
1138 W. CAMBRIDGE CIRCLE DRIVE
 KANSAS CITY, KANSAS 66103

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 WWW.RIC-CONSULT.COM

STANDARD DETAILS

GENERAL NOTES

- APPLICATION. Risers shall be installed to serve all lots or tracts where the sanitary sewer main is below the water table, where the sanitary sewer main depth is greater than 12' below the proposed ground elevation, where the main is adjacent to a pond or wherever service lines would have to cross under storm sewer pipe. Installation of risers because of field conditions shall be as approved by the City Engineer. The location of the risers to serve developed property shall be approved by the property owner and the Construction Engineer.
- MANHOLE STUB RISERS. Manhole stub risers be installed in manholes where locations of manholes will provide satisfactory service connection as determined by the Construction Engineer. The vertical distance between the flowline of the manhole stub and the flowline of the sanitary sewer line out of the manhole shall not exceed 2'. Risers shall be utilized at manholes as indicated in Note 1. Manhole stub riser shall be set such that the top of the stub is not lower than the top of the sanitary sewer line.
- SIZING. Risers shall be sized according to the plans and riser table where risers are indicated by the plans. Where risers are required because of field conditions, the risers shall be 6" diameter for commercial or industrial properties and 4" or 6" diameter for residential properties, based on lot size and sanitary sewer main depth. Sizing of risers shall be approved by the construction Engineer prior to installation.
- RISER MATERIAL. Risers shall be constructed of Schedule 40 PVC Pipe, meeting the requirement of the latest revision of A.S.T.M.. All pipe joints shall be solvent welded. Full body tee shall be SDR 26 PVC pipe.
- ROCK ENCASEMENT. Riser connection to clay pipe sanitary sewers shall be rock encased both ways from the riser centerline. The rock encasement shall extend three feet from the riser centerline or stop at the first sanitary sewer pipe joint within three feet of the riser centerline. Riser connections to PVC Sanitary sewer mains shall be rock encased one foot each way from the riser centerline. Crushed rock shall conform to ASTM C-33, Gradation No. 67, and shall meet all requirements for Portland Cement Concrete pavement Coarse Aggregate, Section 406.2, City of Wichita Standard Specifications.
- BEDDING. Beyond the limits of the rock encasement, bedding around the sanitary sewer riser shall be compacted Pipe Bedding Type 1 or 2. The bedding shall be placed and compacted from the depth of the sanitary sewer main to the top of the sanitary sewer riser pipe. Compacted Pipe Bedding Type 1 or 2 shall be required for all risers whether constructed in vertical wall or sloped wall trenches. Bedding material and construction practices shall be approved by the Construction Engineer prior to installation.
- SUPPORT OF RISERS. Sanitary sewer riser pipe shall be supported during trench backfill. The riser pipe shall be held in a vertical position at all times until trench backfill and compaction has been completed. Contractor's methods for supporting and back filling the riser pipe shall be approved by the Construction Engineer.
- PLUGGING. The ends of the riser pipes and manhole stubs shall be plugged using an airtight solvent welded cap or plug. Cap or plug fittings shall be approved by the Construction Engineer prior to installation. Caps or plugs which do not provide an airtight seal will not be accepted.
- TOP OF THE RISER PIPE. The top elevation of the sanitary sewer riser pipe shall be built per plan elevations, unless otherwise directed by the Construction Engineer, where riser elevations are not shown on the plans, the top of the risers shall be set at an elevation four feet below the proposed ground surface. If ground water is encountered, the top of the riser pipe shall be set at an elevation 2' (min.) above the maximum water table elevation, regardless of the riser elevation shown on the plans.
- MARKING. Locations of the ends of the sanitary sewer riser pipe shall be marked by installing 1" PVC from the top of the riser to a minimum of 4' above the top of finished grade. No. 4 rebar shall be placed centered over the riser from the cap to the existing ground. The 1" PVC pipe shall be wrapped with green colored plastic tape for the full length above ground surface. The green tape shall be 4 mil Polyethylene film with a minimum width of three inches, specifically manufactured for the purpose of identification of underground sewers.
- LOCATION MEASURES. The project inspector shall record and document the location of all risers constructed as measured from the nearest manhole, indicate the direction from the manhole, the direction and distance from the main, riser size, and elevation of the top of the riser in tabular format.
- RISER LOCATION. The riser shall be located per plan if shown. If not shown on the plan, the riser shall be located at the center of the lot, within one foot of the property side of the easement for the lot being served. All riser locations shall be approved by the Construction Engineer prior to installation.
- PAYMENT. "Riser Assembly, Vertical" shall be paid for at the contract unit price per each, which shall be full compensation for all pipe, fittings, marking tape, length of backfill, labor, site restoration, and any other items necessary to complete the work.
"Riser Assembly, Manhole Stub" shall be paid for at the contract unit price per each, which shall be full compensation for all labor material and incidentals necessary to complete the work including all pipe, fittings, rock encasement, and all other items as required and listed for "Riser Assembly, Vertical"



NOTE: RISER PIPE REQUIREMENTS AT MANHOLE STUBS SHALL BE SIMILAR TO THOSE SHOWN ABOVE.



CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

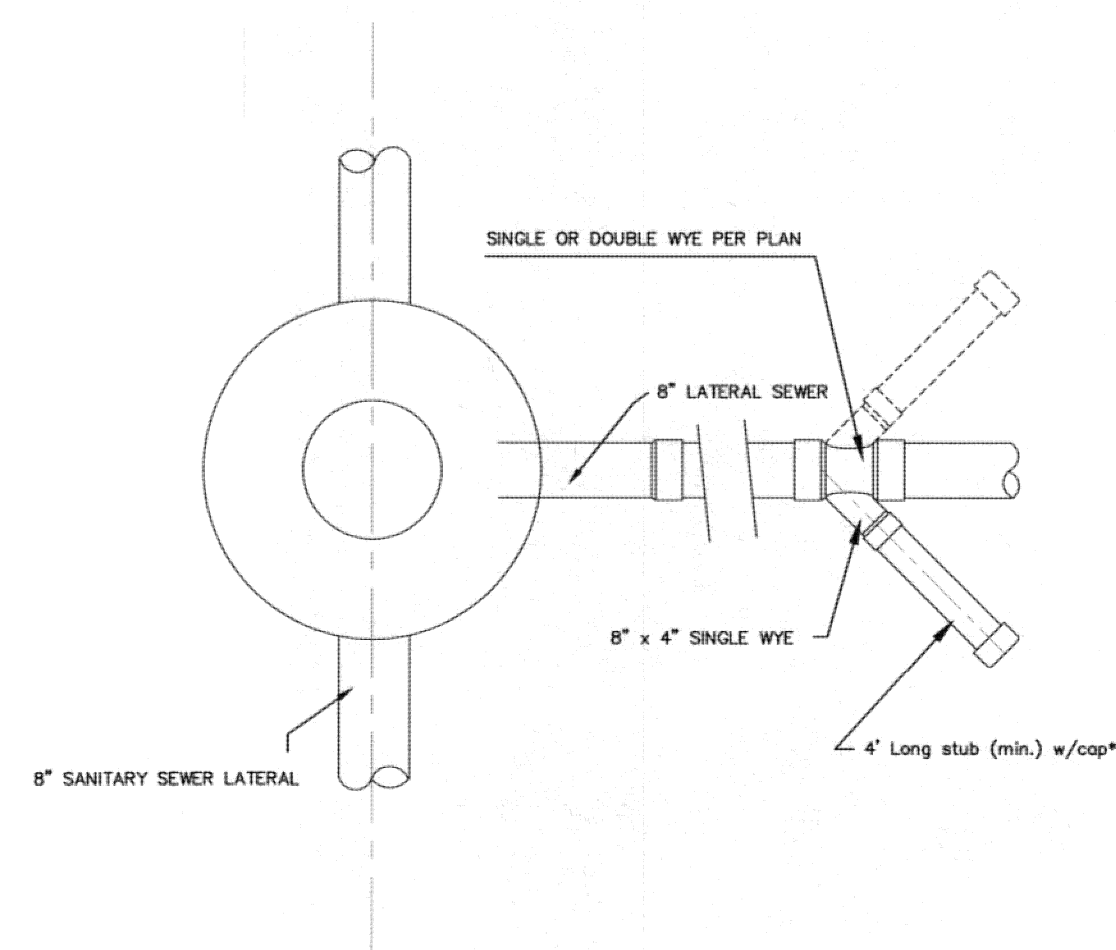
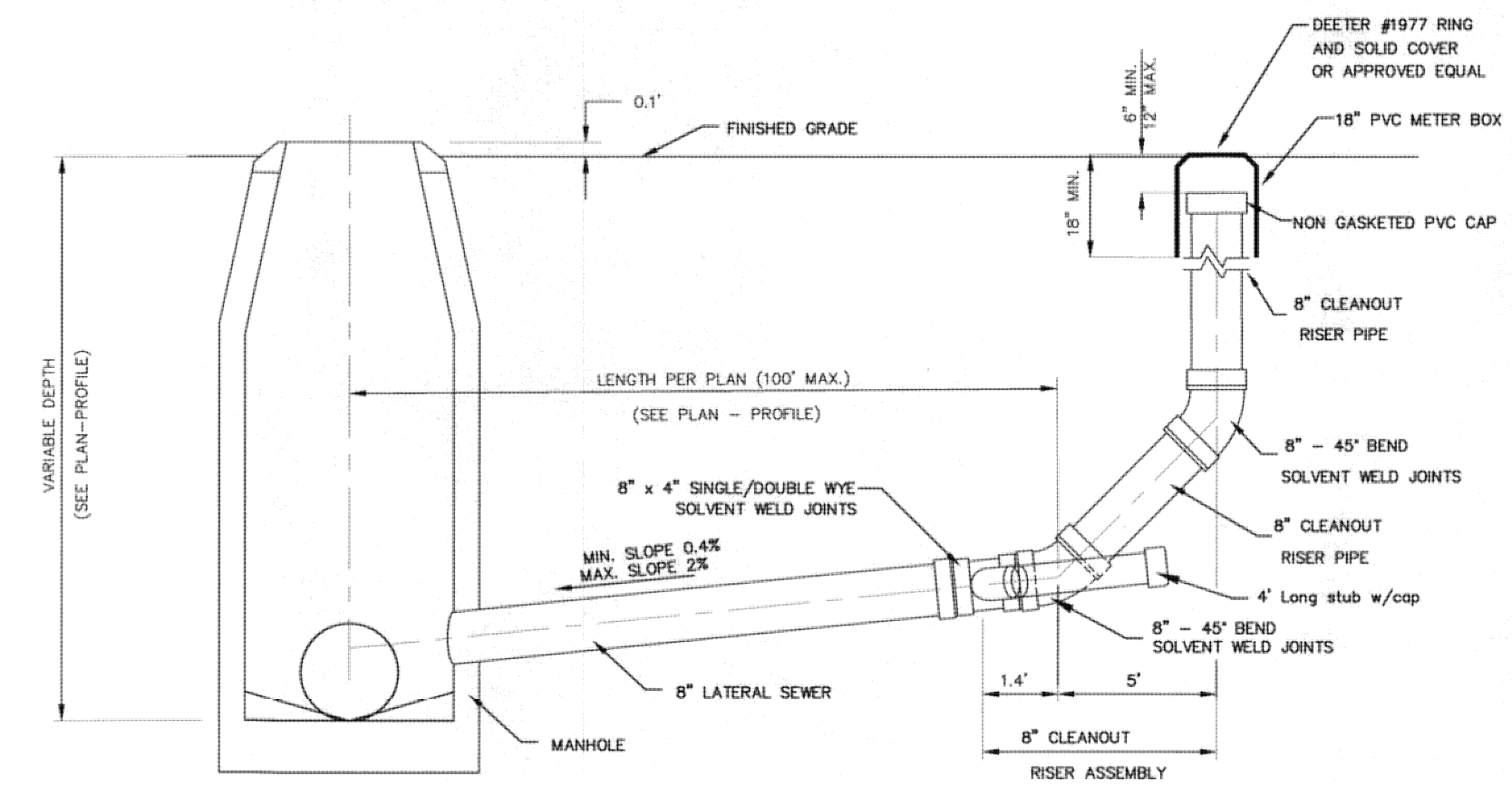
VERTICAL RISER ASSEMBLY SEWER DETAIL
CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER	DCA NUMBER	DATE
		04/2014
CITY ENGINEER'S OFFICE		SHEET
CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501		

SS-103

NO.	BY	DATE	REVISION
6.	JAR	RLC 07/30/15	ISSUED FOR CONSTRUCTION
5.	JAR	RLC 07/16/15	PER REVISED CASING PIPE
4.	JAR	RLC 07/15/15	PER CITY COMMENTS
3.	JAR	RLC 07/08/15	PER CITY COMMENTS
2.	JAR	RLC 06/15/15	PER CITY COMMENTS
1.	JAR	RLC 06/03/15	PER CITY COMMENTS
	JAR	RLC 05/15/15	ORIGINAL SUBMITTAL

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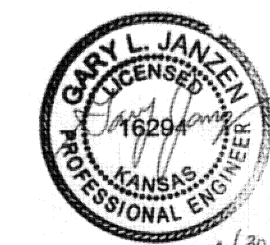
CLEANOUT RISER ASSEMBLY DETAIL
 W/ MANHOLE CONNECTION

* 4" BRANCH (EACH SIDE) TO SERVE AS 4" STUB TEMPORARY CAP UNTIL SERVICE CONNECTION IS REQUIRED. SINGLE OR DOUBLE WYE TO BE USED WHERE INDICATED ON PLAN. WHEN IN GROUNDWATER 4" STUB NEEDS VERTICAL RISER.

** 8" LATERAL TO BE AIR-TESTED UP TO THE TOP OF PVC PIPE, PER STANDARD SPECIFICATIONS.

GENERAL NOTES:

TAPS: NO TAPS ARE PERMITTED BETWEEN THE MANHOLE/TEE AND THE CLEANOUT RISER.
 BEDDING: BEDDING AROUND THE SANITARY SEWER RISER SHALL BE COMPACTED PIPE BEDDING TYPE 2. (TYPE 1 IN GROUNDWATER).
 MATERIAL: RISER AND LATERALS SHALL BE CONSTRUCTED OF SDR-35 PVC PIPE. TEES SHALL BE SDR-26 PIPE, 4" STUBS SHALL BE SCHEDULE 40.
 CONNECTIONS: ALL SERVICE CONNECTIONS MUST BE MADE TO THE WYE PROVIDED.



<p>CITY OF WICHITA PUBLIC WORKS & UTILITIES ENGINEERING DIVISION</p>		CLEANOUT RISER ASSEMBLY DETAIL	
		CITY ENGINEER GARY JANZEN, P.E.	
PROJECT NUMBER	OCA NUMBER	DATE	
		12/2011	
CITY ENGINEER'S OFFICE		SHEET	
CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501			

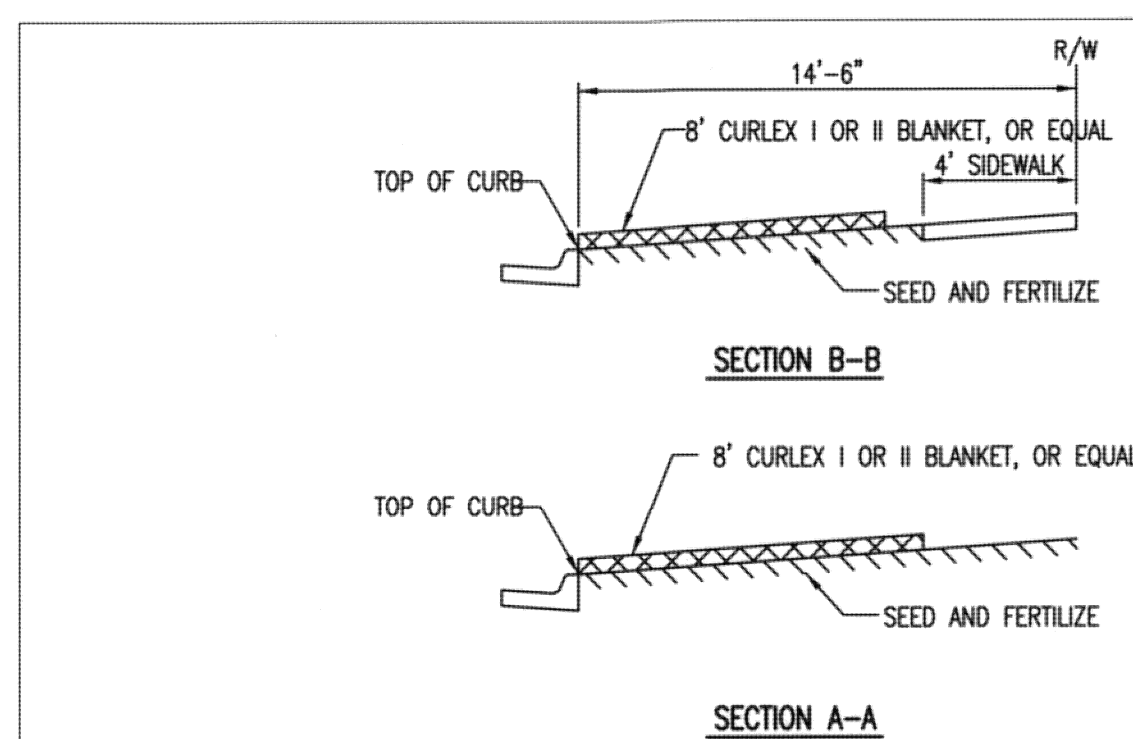
SS-104

NO.	BY	DATE	REVISION
6.	JAR	07/30/15	ISSUED FOR CONSTRUCTION
5.	JAR	07/16/15	PER REVISED CASING PIPE
4.	JAR	07/15/15	PER CITY COMMENTS
3.	JAR	07/08/15	PER CITY COMMENTS
2.	JAR	06/18/15	PER CITY COMMENTS
1.	JAR	06/03/15	PER CITY COMMENTS
	JAR	05/15/15	ORIGINAL SUBMITTAL

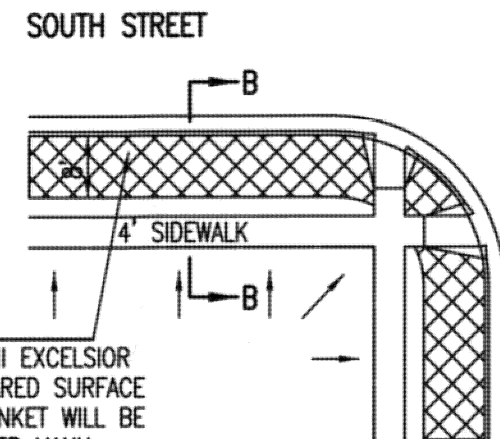
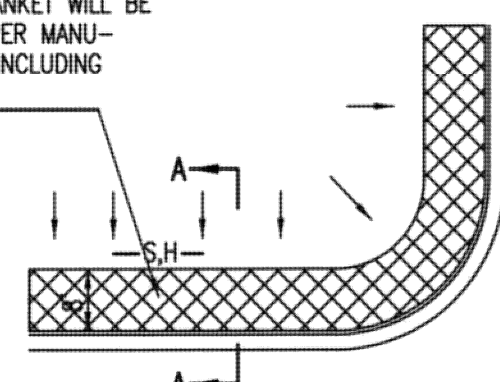
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INSTALL 8" WIDE CURLEX I OR II EXCELSIOR BLANKET, OR EQUAL, ON PREPARED SURFACE BACK OF CURB. EDGE OF BLANKET WILL BE AT BACK OF CURB. INSTALL PER MANUFACTURER'S RECOMMENDATION, INCLUDING STAPLES. (SEE DETAIL)

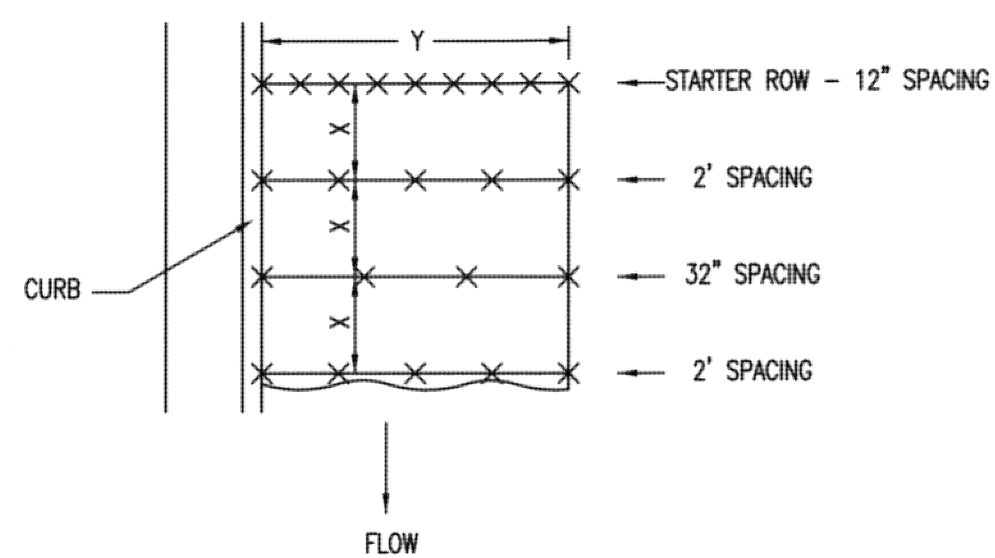


INSTALL 8" WIDE CURLEX I OR II EXCELSIOR BLANKET, OR EQUAL, ON PREPARED SURFACE BACK OF CURB. EDGE OF BLANKET WILL BE AT BACK OF CURB. INSTALL PER MANUFACTURER'S RECOMMENDATION, INCLUDING STAPLES. (SEE DETAIL)

GENERAL NOTES

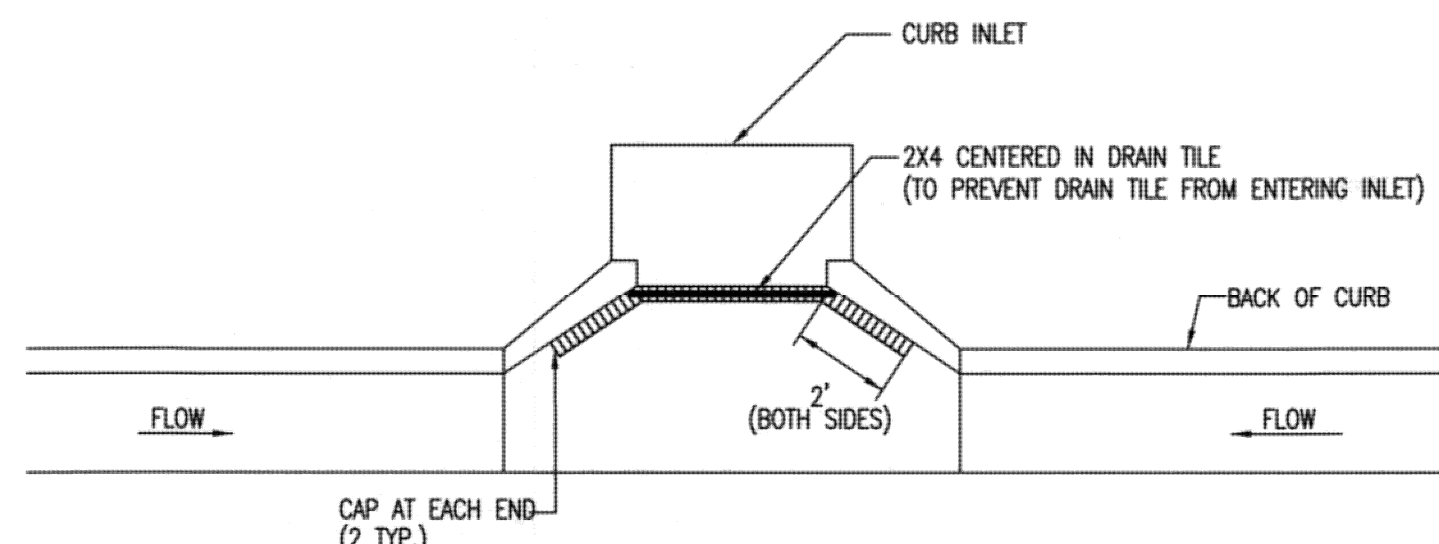
- EXCELSIOR MAT TO BE INSTALLED WHEN SOD IS NOT SPECIFIED ON PROJECT.
- EXCELSIOR BLANKET TO BE INSTALLED OVER SEED AND FERTILIZER, AS SPECIFIED IN THE PROJECT SPECIFICATIONS.
- AFTER INSTALLATION OF EXCELSIOR BLANKET, AT LOCATIONS WHERE CONCENTRATED FLOW CARRIES SEDIMENT OVER THE CURB AND INTO THE GUTTER, SUPPLEMENTAL EROSION CONTROL DEVICES WILL BE INSTALLED BY THE CONTRACTOR AS NEEDED, TO FIX THE PROBLEM.

BACK OF CURB PROTECTION DETAIL



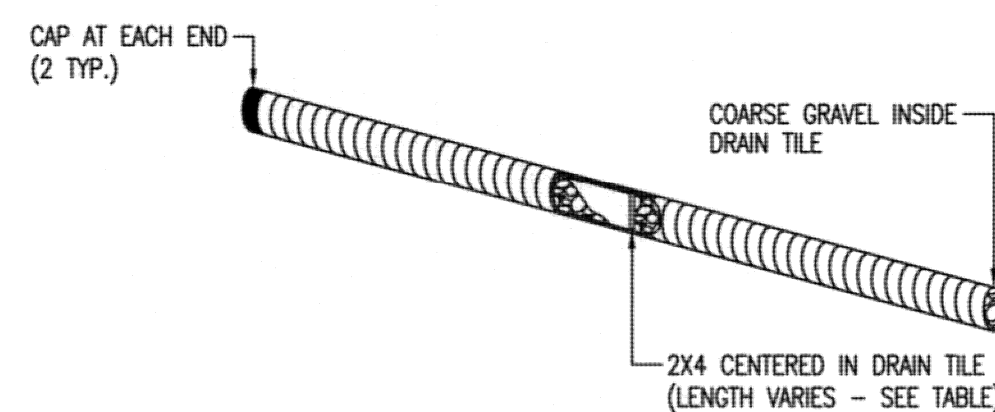
STAPLE PATTERN
NOTES: USE 6" SEAM OVERLAP
(X & Y = RECOMMENDED BY MANUFACTURE)

DETAILS FOR APPROVED EROSION CONTROL MAT

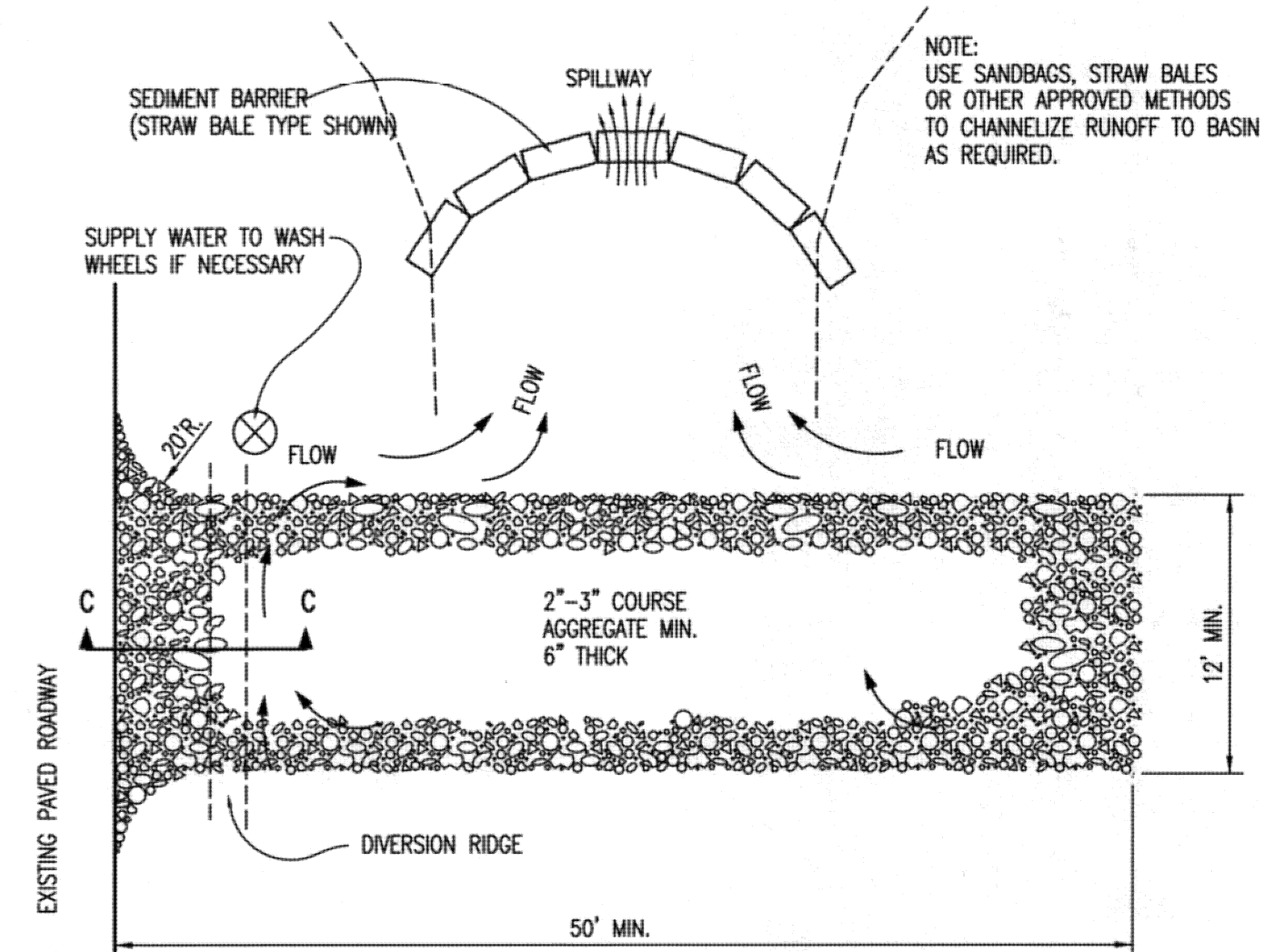
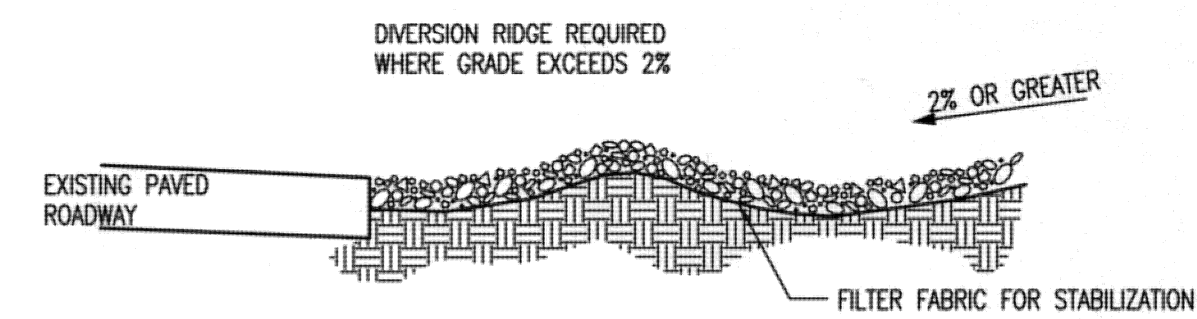


NOTE: PLACE 4" PERFORATED PVC PIPE, FILLED WITH 1/2"-1" DIA. GRAVEL, IN FRONT OF CURB INLET AS SHOWN.

2X4 LENGTH	INLET TYPE	INLET OPENING
5'-6"	1-A	5'-0"
10'-6"	1-A	10'-0"
15'-6"	1-A	15'-0"



CURB INLET PROTECTION
4" PERFORATED PIPE W/ GRAVEL

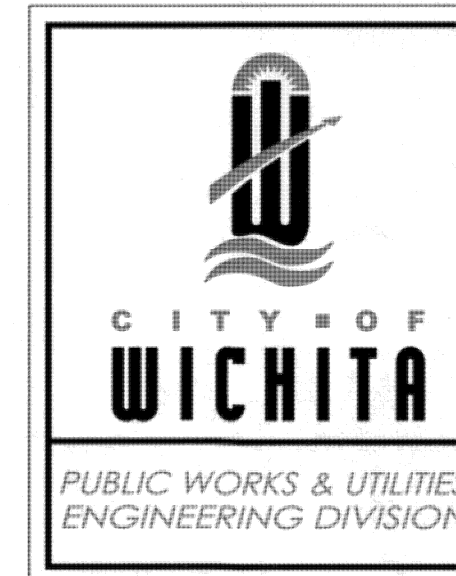


STABILIZED CONSTRUCTION ENTRANCE

GENERAL NOTES

- THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.
- WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.
- WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN, AS SHOWN ABOVE.
- DRIVE ENTRANCES ONTO RESIDENTIAL LOTS WILL NOT BE REQUIRED TO HAVE THE SEDIMENT BARRIER SHOWN, BUT WHEEL WASHING MAY BE REQUIRED IF STABILIZED ENTRANCE IS NOT SUFFICIENT TO KEEP MUD FROM BEING TRACKED ONTO ADJACENT STREET. ENTRANCE SHALL EXTEND FROM BACK OF CURB TO DWELLING.

REVISION DATE: MAY 2013



BACK OF CURB PROTECTION, CURB INLET PROTECTION AND CONSTRUCTION ENTRANCE

CITY ENGINEER
GARY JANZEN, P.E.
PROJECT NUMBER OCA NUMBER DATE
CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

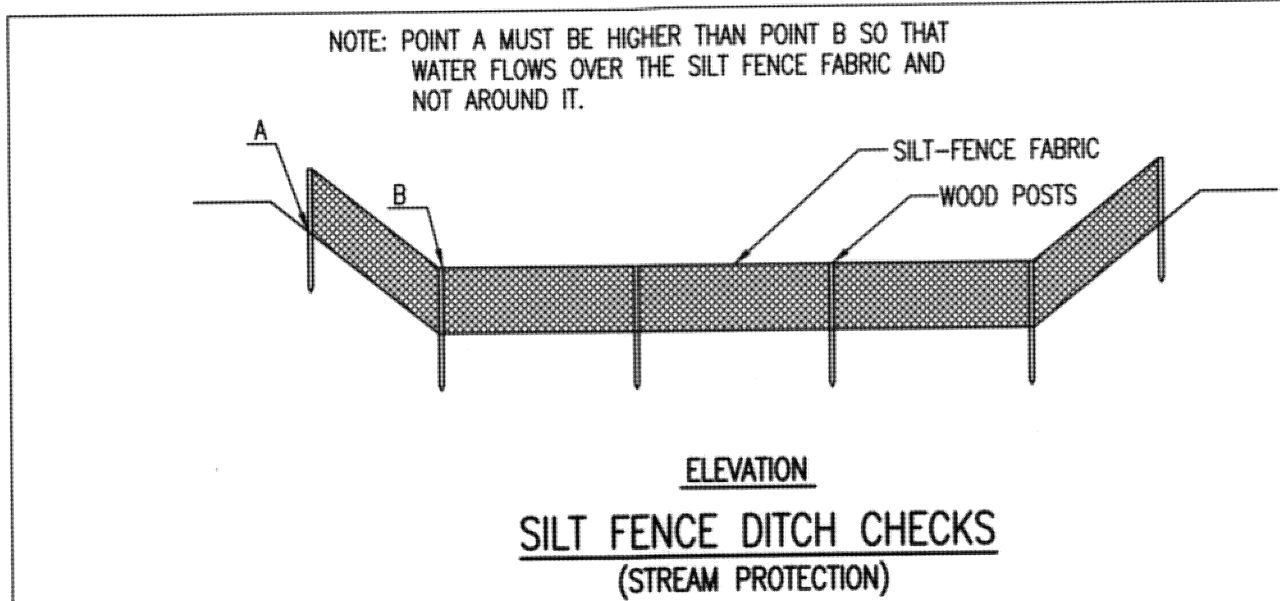


05/2013

SW-501

NO.	BY	DATE	REVISION
6.	JAR	07/30/15	ISSUED FOR CONSTRUCTION
5.	JAR	07/16/15	PER REVISED CASING PIPE
4.	JAR	07/15/15	PER CITY COMMENTS
3.	JAR	07/08/15	PER CITY COMMENTS
2.	JAR	06/18/15	PER CITY COMMENTS
1.	JAR	06/03/15	PER CITY COMMENTS
	JAR	05/15/15	ORIGINAL SUBMITTAL

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ELEVATION
SILT FENCE DITCH CHECKS
(STREAM PROTECTION)

MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. SILT FENCE FABRIC SHOULD BE ATTACHED TO THE WOODEN POSTS WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

PLACE SILT FENCE IN DITCHES WHERE IT IS UNLIKELY THAT IT WILL BE OVERTOPPED. WATER SHOULD FLOW THROUGH A SILT FENCE DITCH CHECK, NOT OVER IT. SILT FENCE DITCH CHECKS OFTEN FAIL WHEN OVERTOPPED. SILT FENCE DITCH CHECKS SHOULD BE PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. THE SILT FENCE SHOULD EXTEND FAR ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE FENCE IS HIGHER THAN THE TOP OF THE LOW POINT OF THE FENCE. THIS PREVENTS WATER FROM FLOWING AROUND THE CHECK. SILT FENCE DITCH CHECKS SHOULD NOT BE PLACED IN DITCHES WHERE HIGH FLOWS ARE EXPECTED. ROCK CHECKS SHOULD BE USED INSTEAD. SILT FENCE SHOULD BE PLACED IN DITCHES WITH SLOPES OF 6% OR LESS. FOR SLOPES STEEPER THAN 6%, ROCK CHECKS SHOULD BE USED.

THE FOLLOWING TABLE PROVIDES CHECK SPACING FOR A GIVEN DITCH GRADE:

DITCH CHECK DITCH GRADE (%)	SPACING CHECK SPACING (FEET)
0.5	200
1.0	200
2.0	100
3.0	65
4.0	50
5.0	40
6.0	30

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH PERPENDICULAR TO THE DITCH FLOWLINE THAT IS AT LEAST 12" DEEP BY 6" WIDE. EXTEND THE TRENCH IN A STRAIGHT LINE ALONG THE ENTIRE LENGTH OF THE PROPOSED DITCH CHECK. PLACE THE SOIL ON THE UPSLOPE SIDE OF THE TRENCH FOR LATER USE. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC ON THE DOWNSLOPE SIDE OF THE TRENCH. PLACE THE EDGE OF THE FABRIC IN THE TRENCH STARTING AT THE TOP UPSLOPE EDGE OF THE TRENCH. LINE TWO SIDES OF THE TRENCH WITH THE FABRIC AS SHOWN ON DETAIL. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT FENCE FABRIC SHOULD REMAIN EXPOSED. LAY THE EXPOSED SILT FENCE ON THE UPSLOPE SIDE OF THE TRENCH TO CLEAR AN AREA FOR DRIVING IN THE POSTS. JUST DOWNSLOPE OF THE TRENCH, DRIVE POSTS INTO THE GROUND TO A DEPTH OF AT LEAST 24". PLACE POSTS NO MORE THAN 4' APART. ATTACH THE SILT FENCE TO THE ANCHORED POST WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

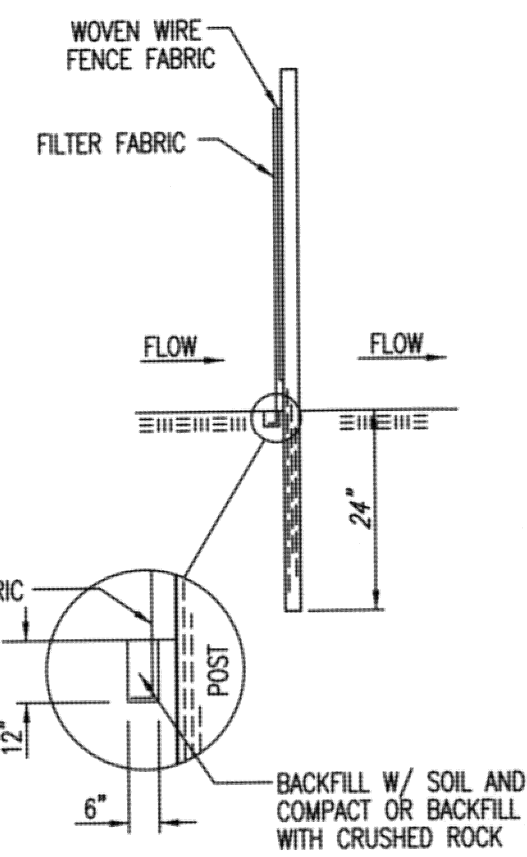
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WATER SHOULD FLOW THROUGH A SILT FENCE DITCH CHECK—NOT OVER IT. PLACE SILT FENCE IN DITCHES WHERE IT IS UNLIKELY THAT IT WILL BE OVERTOPPED. SILT FENCE INSTALLATIONS QUICKLY DETERIORATE WHEN WATER OVERTOPS THEM. DO NOT PLACE SILT FENCE POSTS ON THE UPSLOPE SIDE OF THE SILT FENCE FABRIC. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESTRICTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT PLACE A SILT FENCE DITCH CHECK DIRECTLY IN FRONT OF A CULVERT OUTLET. IT WILL NOT STAND UP TO THE CONCENTRATED FLOW. DO NOT PLACE SILT FENCE DITCH CHECKS IN DITCHES THAT WILL LIKELY EXPERIENCE HIGH FLOWS. THEY WILL NOT STAND UP TO CONCENTRATED FLOW. FOLLOW PRESCRIBED DITCH CHECK SPACING GUIDELINES. IF SPACING GUIDELINES ARE EXCEEDED, EROSION WILL OCCUR BETWEEN THE DITCH CHECKS. DO NOT ALLOW WATER TO FLOW AROUND THE DITCH CHECK. MAKE SURE THAT THE DITCH CHECK IS LONG ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE FENCE IS HIGHER THAN THE LOW POINT ON THE TOP OF THE FENCE. DO NOT PLACE SILT FENCE DITCH CHECKS IN CHANNELS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE CHECK IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT.

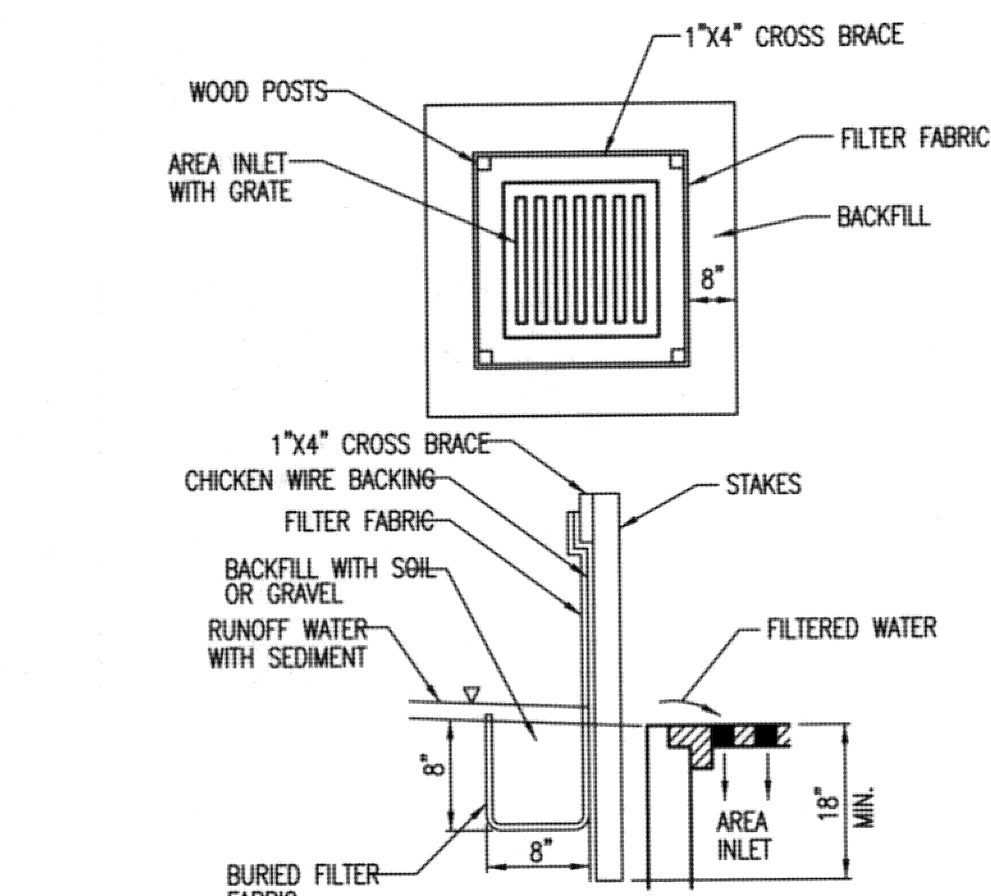
INSPECTION AND MAINTENANCE:

SILT FENCE DITCH CHECKS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW AROUND THE DITCH CHECK?
- DOES WATER FLOW UNDER THE DITCH CHECK?
- DOES THE SILT FENCE SAG EXCESSIVELY?
- HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE DITCH CHECK?



ANCHOR TRENCH DETAIL



SILT FENCE BARRIERS FOR AREA INLETS
(INLET PROTECTION)

MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE WIRE OR POLYMERIC MESH BACKING USED TO HELP SUPPORT THE SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. THE MATERIAL USED TO FRAME THE TOPS OF THE POSTS SHOULD BE 1" BY 4" BOARDS. SILT FENCE FABRIC AND SUPPORT BACKING SHOULD BE ATTACHED TO THE WOODEN POSTS AND FRAME WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

PLACE A SILT FENCE DROP INLET BARRIER IN A LOCATION WHERE IT IS UNLIKELY TO BE OVERTOPPED. WATER SHOULD FLOW THROUGH SILT FENCE, NOT OVER IT. SILT FENCE BARRIERS FOR AREA INLETS OFTEN FAIL WHEN REPEATEDLY OVERTOPPED. WHEN USED AS A BARRIER FOR AREA INLETS, SILT FENCE FABRIC AND POSTS MUST BE SUPPORTED AT THE TOP BY A WOODEN FRAME. WHEN A SILT FENCE BARRIER FOR AREA INLETS IS LOCATED NEAR AN INLET THAT HAS STEEP APPROACH SLOPES, THE STORAGE CAPACITY BEHIND THE BARRIER IS DRASTICALLY REDUCED. TIMELY REMOVAL OF SEDIMENT MUST OCCUR FOR A BARRIER TO OPERATE PROPERLY IN THIS LOCATION.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH AROUND THE PERIMETER OF THE AREA INLET THAT IS AT LEAST 8" DEEP BY 8" WIDE. DRIVE POSTS TO A DEPTH OF AT LEAST 18" AROUND THE PERIMETER OF THE AREA INLET. THE DISTANCE BETWEEN POSTS SHOULD BE 4' OR LESS. IF THE DISTANCE BETWEEN TWO ADJACENT CORNER POSTS IS MORE THAN 4', ADD ANOTHER POST(S) BETWEEN THEM. CONNECT THE TOPS OF ALL THE POSTS WITH A WOODEN FRAME MADE OF 1" BY 4" BOARDS. USE NAILS OR SCREWS FOR FASTENING. ATTACH THE WIRE OR POLYMERIC-MESH BACKING TO THE OUTSIDE OF THE POST/FRAME STRUCTURE WITH STAPLES, WIRE, ZIP TIES, OR NAILS. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC LONG ENOUGH TO WRAP AROUND THE PERIMETER OF THE AREA INLET. ADD MORE LENGTH FOR OVERLAPPING THE FABRIC JOINT. PLACE THE EDGE OF THE FABRIC IN THE TRENCH, STARTING AT THE OUTSIDE EDGE OF THE TRENCH. LINE ALL THREE SIDES OF THE TRENCH WITH THE FABRIC. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT FENCE FABRIC SHOULD REMAIN EXPOSED. ATTACH THE SILT FENCE TO THE OUTSIDE OF THE POST/FRAME STRUCTURE WITH STAPLES, WIRE, ZIP TIES, OR NAILS. THE JOINT SHOULD BE OVERLAPPED TO THE NEXT POST.

NOTE: WHEN A SILT FENCE BARRIER FOR AREA INLET IS PLACED IN A SHALLOW MEDIAN DITCH, MAKE SURE THAT THE TOP OF THE BARRIER IS NOT HIGHER THAN THE PAVED ROAD. IN THIS CONFIGURATION, WATER MAY SPREAD ONTO THE ROADWAY CAUSING A HAZARDOUS CONDITION.

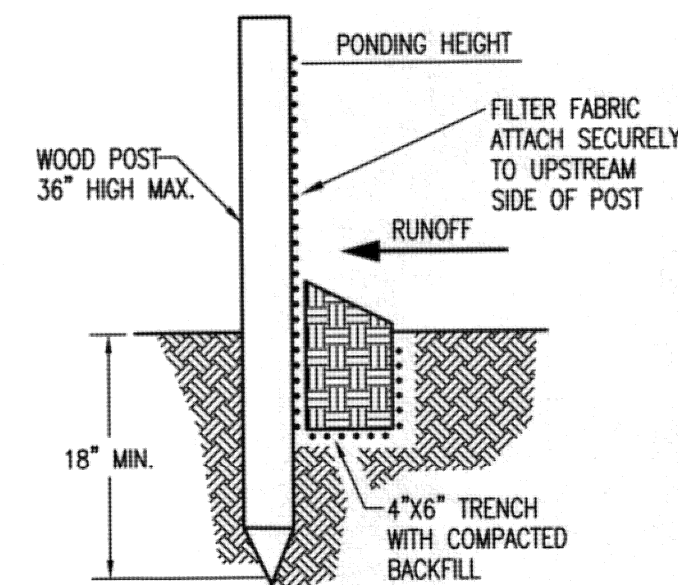
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WATER SHOULD FLOW THROUGH A SILT FENCE BARRIER FOR AREA INLET—NOT OVER IT. PLACE A SILT FENCE BARRIER FOR AREA INLET IN A LOCATION WHERE IT IS UNLIKELY TO BE OVERTOPPED. SILT FENCE BARRIER FOR AREA INLETS OFTEN FAIL WHEN REPEATEDLY OVERTOPPED. DO NOT PLACE POSTS ON THE OUTSIDE OF THE SILT FENCE BARRIER FOR AREA INLET. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESTRICTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT INSTALL SILT FENCE BARRIER FOR AREA INLETS WITHOUT FRAMING THE TOP OF THE POSTS. THE CORNER POSTS AROUND AREA INLETS ARE STRESSED IN TWO DIRECTIONS WHEREAS A NORMAL SILT FENCE IS ONLY STRESSED IN ONE DIRECTION. THIS ADDED STRESS REQUIRES MORE SUPPORT.

INSPECTION AND MAINTENANCE:

SILT FENCE BARRIER FOR AREA INLETS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW UNDER THE SILT FENCE?
- DOES THE SILT FENCE SAG EXCESSIVELY?
- HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE AREA INLET BARRIER?



SILT FENCE BARRIERS

MATERIAL SPECIFICATION:

SILT FENCE FABRIC SHOULD CONFORM TO THE AASHTO M288 96 SILT FENCE SPECIFICATION. THE POSTS USED TO SUPPORT THE SILT FENCE FABRIC SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. SILT FENCE FABRIC SHOULD BE ATTACHED TO THE WOODEN POSTS WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

PLACEMENT:

A SLOPE BARRIER SHOULD BE USED AT THE TOE OF A SLOPE WHEN A DITCH DOES NOT EXIST. THE SLOPE BARRIER SHOULD BE PLACED ON NEARLY LEVEL GROUND 5' TO 10' AWAY FROM THE TOE OF A SLOPE. THE BARRIER IS PLACED AWAY FROM THE TOE OF THE SLOPE TO PROVIDE ADEQUATE STORAGE FOR SETTLING OUT SEDIMENT. WHEN PRACTICABLE, SILT FENCE SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. SILT FENCE SLOPE BARRIERS CAN ALSO BE PLACED ALONG RIGHT-OF-WAY FENCE LINES TO KEEP SEDIMENT FROM CROSSING ONTO ADJACENT PROPERTY. WHEN PLACED IN THIS MANNER, THE SLOPE BARRIER WILL NOT LIKELY FOLLOW CONTOURS.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH THE LENGTH OF THE PLANNED SLOPE BARRIER THAT IS 6" DEEP BY 4" WIDE. MAKE SURE THAT THE TRENCH IS EXCAVATED ALONG A SINGLE CONTOUR. WHEN PRACTICABLE, SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. PLACE THE SOIL ON THE UPSLOPE SIDE OF THE TRENCH FOR LATER USE. ROLL OUT A CONTINUOUS LENGTH OF SILT FENCE FABRIC ON THE DOWNSLOPE SIDE OF THE TRENCH. PLACE THE EDGE OF THE FABRIC IN THE TRENCH STARTING AT THE TOP UPSLOPE EDGE. LINE ALL THREE SIDES OF THE TRENCH WITH THE FABRIC. BACKFILL OVER THE FABRIC IN THE TRENCH WITH THE EXCAVATED SOIL AND COMPACT. AFTER FILLING THE TRENCH, APPROXIMATELY 24" TO 36" OF SILT-FENCE FABRIC SHOULD REMAIN EXPOSED. LAY THE EXPOSED SILT FENCE UPSLOPE OF THE TRENCH TO CLEAR AN AREA FOR DRIVING IN THE POSTS. JUST DOWNSLOPE OF THE TRENCH, DRIVE POSTS INTO THE GROUND TO A DEPTH OF AT LEAST 18". PLACE POSTS NO MORE THAN 4' APART. ATTACH THE SILT FENCE TO THE ANCHORED POST WITH STAPLES, WIRE, ZIP TIES, OR NAILS.

LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

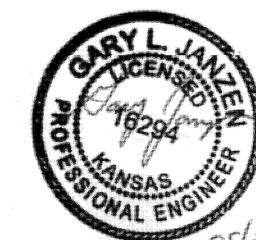
WHEN PRACTICABLE, DO NOT PLACE SILT FENCE SLOPE BARRIERS ACROSS CONTOURS. SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. WHEN THE FLOW CONCENTRATES, IT OVERTOPS THE BARRIER AND THE SILT FENCE SLOPE BARRIER QUICKLY DETERIORATES. DO NOT PLACE SILT-FENCE POSTS ON THE UPSLOPE SIDE OF THE SILT FENCE FABRIC. IN THIS CONFIGURATION, THE FORCE OF THE WATER IS NOT RESTRICTED BY THE POSTS, BUT ONLY BY THE STAPLES (WIRE, ZIP TIES, NAILS, ETC.). THE SILT FENCE WILL RIP AND FAIL. DO NOT PLACE SILT FENCE SLOPE BARRIERS IN AREAS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE BARRIER IS NOT SUFFICIENTLY ANCHORED, IT WILL WASH OUT. SILT FENCE SLOPE BARRIERS MUST BE DUG INTO THE GROUND—SILT FENCE AT GROUND LEVEL DOES NOT WORK BECAUSE WATER WILL FLOW UNDERNEATH.

INSPECTION AND MAINTENANCE:

SILT FENCE SLOPE BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- ARE THERE ANY POINTS ALONG THE SLOPE BARRIER WHERE WATER IS CONCENTRATING?
- DOES WATER FLOW UNDER THE SLOPE BARRIER?
- DOES THE SILT FENCE SAG EXCESSIVELY?
- HAS THE SILT FENCE TORN OR BECOME DETACHED FROM THE POSTS?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE SLOPE BARRIER?

REVISION DATE: MAY 2015



SILT FENCE DITCH CHECK AND BARRIER DETAILS

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER	OCA NUMBER	DATE
CITY ENGINEER'S OFFICE		SHEET
CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1420 (316) 268-4501		

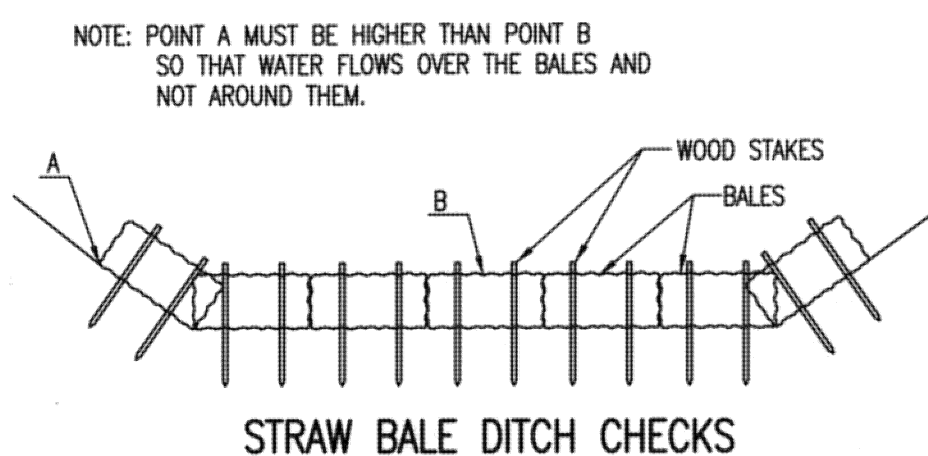
SW-502

ISSUED FOR CONSTRUCTION	PER REVISED CABING PIPE	PER CITY COMMENTS	PER CITY COMMENTS	PER CITY COMMENTS	ORIGINAL SUBMITTAL	REVISION
6. JAR	RLC	07/30/15				
5. JAR	RLC	07/16/15				
4. JAR	RLC	07/15/15				
3. JAR	RLC	07/08/15				
2. JAR	RLC	06/15/15				
1. JAR	RLC	06/09/15				
	JAR	PHC	06/15/15			
	JAR	PHC	06/15/15			
						NO.
						DATE

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KANSAS CITY, KANSAS 66103



STRAW BALE DITCH CHECKS

MATERIAL SPECIFICATION:

BALE DITCH CHECKS MAY BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. OPTIONAL: THE DOWNSTREAM SCOUR APRON SHOULD BE CONSTRUCTED OF A DOUBLE-NETTED STRAW EROSION-CONTROL BLANKET AT LEAST 6' WIDE. OPTIONAL: THE METAL LANDSCAPE STAPLES USED TO ANCHOR THE EROSION-CONTROL BLANKET SHOULD BE AT LEAST 8" LONG.

PLACEMENT:

BALE DITCH CHECKS SHOULD BE PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. THE DITCH CHECK SHOULD EXTEND FAR ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE CHECK IS HIGHER THAN THE TOP OF THE LOWEST CENTER BALE. THIS PREVENTS WATER FROM FLOWING AROUND THE CHECK. STRAW BALE DITCH CHECKS SHOULD NOT BE PLACED IN DITCHES WHERE HIGH FLOWS ARE EXPECTED. ROCK CHECKS SHOULD BE USED INSTEAD. BALES SHOULD BE PLACED IN DITCHES WITH SLOPES OF 6% OR LESS. FOR SLOPES STEEPER THAN 6%, ROCK CHECKS SHOULD BE USED. THE FOLLOWING TABLE PROVIDES CHECK SPACING FOR A GIVEN DITCH GRADE:

DITCH CHECK SPACING	DITCH GRADE (%)	CHECK SPACING (FEET)
0.5	200	
1.0	200	
2.0	100	
3.0	65	
4.0	50	
5.0	40	
6.0	30	

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH PERPENDICULAR TO THE DITCH FLOWLINE THAT IS 4" DEEP AND A BALE'S WIDTH WIDE. EXTEND THE TRENCH IN A STRAIGHT LINE ALONG THE ENTIRE LENGTH OF THE PROPOSED DITCH CHECK. PLACE THE SOIL ON THE UPSTREAM SIDE OF THE TRENCH-IT WILL BE USED LATER. OPTIONAL: ON THE DOWNSTREAM SIDE OF THE TRENCH, ROLL OUT A LENGTH OF EROSION-CONTROL BLANKET (SCOUR APRON) EQUAL TO THE LENGTH OF THE TRENCH. PLACE THE UPSTREAM EDGE OF THE EROSION-CONTROL BLANKET ALONG THE BOTTOM UPSTREAM EDGE OF THE TRENCH. THE EROSION CONTROL BLANKET SHOULD BE ANCHORED IN THE TRENCH WITH ONE ROW OF 8" LANDSCAPE STAPLES PLACED ON 18" CENTERS. THE REMAINDER OF THE EROSION-CONTROL BLANKET (THE PORTION THAT IS NOT LYING IN THE TRENCH) WILL SERVE AS THE DOWNSTREAM SCOUR APRON. THIS SECTION OF THE BLANKET SHOULD BE ANCHORED TO THE GROUND WITH 8" LANDSCAPE STAPLES PLACED AROUND THE PERIMETER OF THE BLANKET ON 18" CENTERS. THE REMAINDER OF THE BLANKET SHOULD BE ANCHORED USING TWO EVENLY SPACED ROWS OF 8" LANDSCAPE STAPLES ON 18" CENTERS PLACED PERPENDICULAR TO THE FLOWLINE OF THE DITCH. PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE ALONG THE CENTERLINE OF THE DITCH CHECK, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND. ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE UPSTREAM SIDE OF THE CHECK AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP AND EXTEND UPSTREAM NO MORE THAN 24".

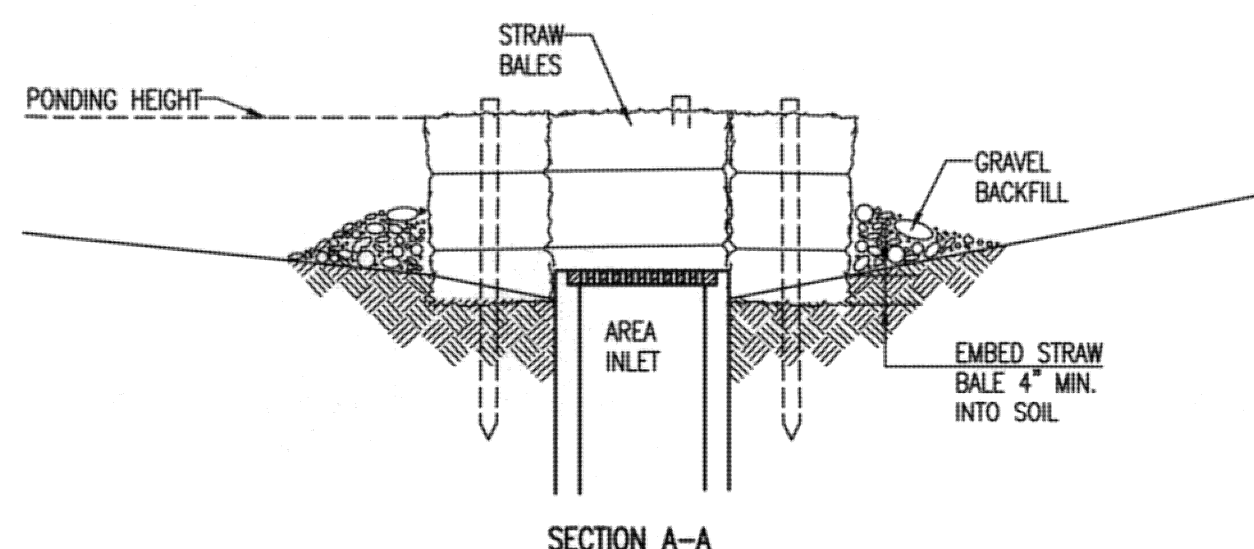
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

DO NOT PLACE A BALE DITCH CHECK DIRECTLY IN FRONT OF A CULVERT OUTLET. IT WILL NOT STAND UP TO THE CONCENTRATED FLOW. DO NOT PLACE BALE DITCH CHECKS IN DITCHES THAT WILL LIKELY EXPERIENCE HIGH FLOWS. THEY WILL NOT STAND UP TO CONCENTRATED FLOW. FOLLOW PRESCRIBED DITCH-CHECK SPACING GUIDELINES. IF SPACING GUIDELINES ARE EXCEEDED, EROSION WILL OCCUR BETWEEN THE DITCH CHECKS. DO NOT ALLOW WATER TO FLOW AROUND THE DITCH CHECK. MAKE SURE THAT THE DITCH CHECK IS LONG ENOUGH SO THAT THE GROUND LEVEL AT THE ENDS OF THE CHECK IS HIGHER THAN THE TOP OF THE LOWEST CENTER BALE. DO NOT PLACE BALE DITCH CHECKS IN CHANNELS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE CHECK IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT. BALE DITCH CHECKS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE CHECK.

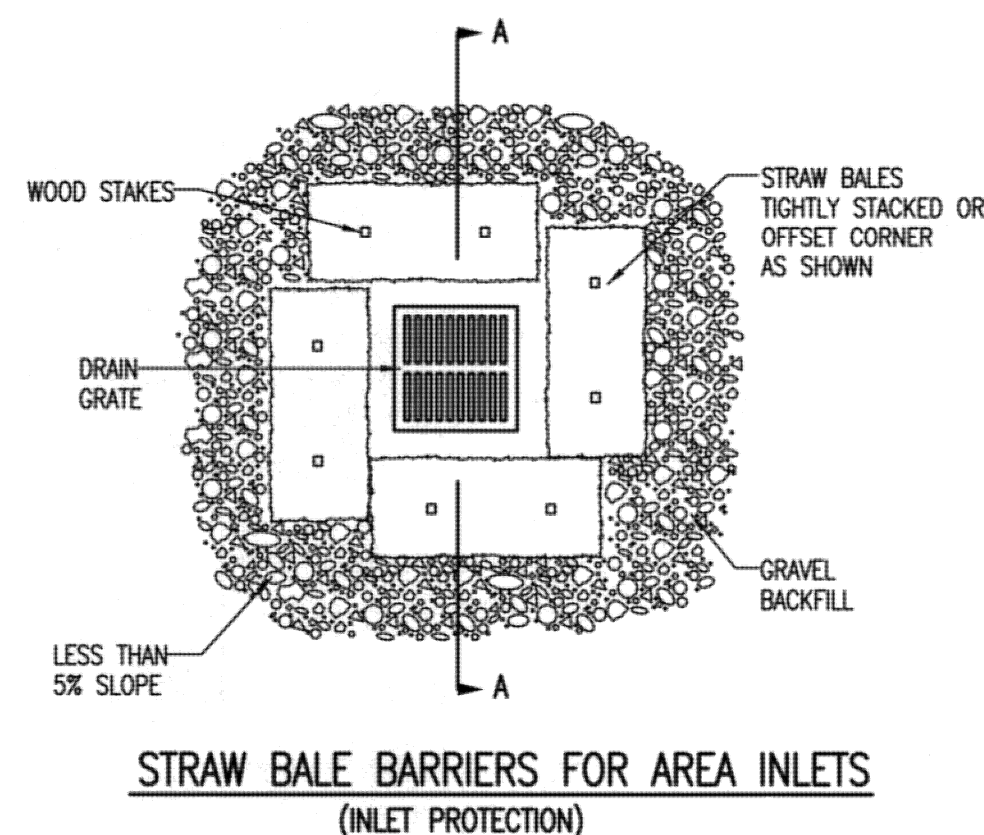
INSPECTION AND MAINTENANCE:

BALE DITCH CHECKS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW AROUND THE DITCH CHECK?
- DOES WATER FLOW UNDER THE DITCH CHECK?
- DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
- ARE ANY BALES AND/OR SCOUR APRONS (OPTIONAL) DISLODGED?
- ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE DITCH CHECK?



SECTION A-A



STRAW BALE BARRIERS FOR AREA INLETS (INLET PROTECTION)

MATERIAL SPECIFICATION:

BALE AREA INLET BARRIERS SHOULD BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. TWINE SHOULD BE USED TO BIND BALES. THE USE OF WIRE BINDING IS PROHIBITED BECAUSE IT DOES NOT BIODEGRADE READILY.

PLACEMENT:

BALE AREA INLET BARRIERS SHOULD BE PLACED DIRECTLY AROUND THE PERIMETER OF A DROP INLET. WHEN A BALE AREA INLET BARRIER IS LOCATED NEAR AN INLET THAT HAS STEEP APPROACH SLOPES, THE STORAGE CAPACITY BEHIND THE BARRIER IS DRASTICALLY REDUCED. TIMELY REMOVAL OF SEDIMENT MUST OCCUR FOR A BARRIER TO OPERATE PROPERLY IN THIS LOCATION.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH AROUND THE PERIMETER OF THE AREA INLET THAT IS AT LEAST 4" DEEP BY A BALE'S WIDTH WIDE. PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. SOME BALES MAY NEED TO BE SHORTENED TO FIT INTO THE TRENCH AROUND THE AREA INLET. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND. ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE RECEIVING SIDE OF THE BARRIER AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP. NOTE: WHEN A BALE AREA INLET BARRIER IS PLACED IN A SHALLOW MEDIAN DITCH, MAKE SURE THAT THE TOP OF THE BARRIER IS NOT HIGHER THAN THE PAVED ROAD. IN THIS CONFIGURATION, WATER MAY SPREAD ONTO THE ROADWAY CAUSING A HAZARDOUS CONDITION.

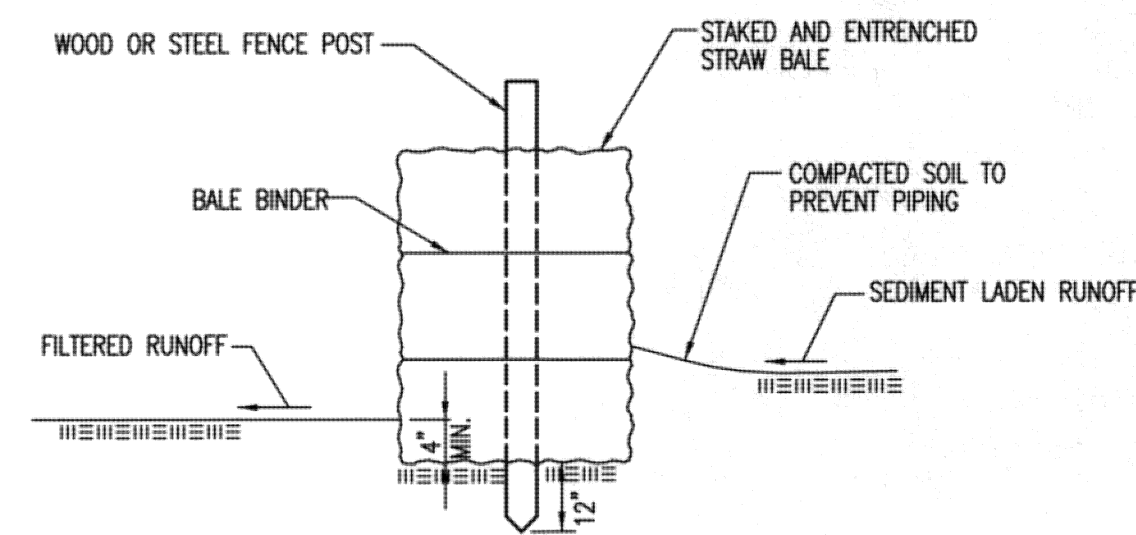
LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

BALES SHOULD BE PLACED DIRECTLY AGAINST THE PERIMETER OF THE AREA INLET. THIS ALLOWS OVERTOPPING WATER TO FLOW DIRECTLY INTO THE INLET INSTEAD OF ONTO NEARBY SOIL CAUSING SCOUR. BALE AREA INLET BARRIERS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE BARRIER.

INSPECTION AND MAINTENANCE:

BALE AREA INLET BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- DOES WATER FLOW UNDER THE AREA INLET BARRIER?
- DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
- ARE ANY BALES DISLODGED?
- ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE AREA INLET BARRIER?



STRAW BALE BARRIERS

MATERIAL SPECIFICATION:

BALE SLOPE BARRIERS MAY BE CONSTRUCTED OF WHEAT STRAW, OAT STRAW, PRAIRIE HAY, OR BROMEGRASS HAY THAT IS FREE OF WEEDS DECLARED NOXIOUS BY THE KANSAS STATE BOARD OF AGRICULTURE. THE STAKES USED TO ANCHOR THE BALES SHOULD BE A HARDWOOD MATERIAL WITH THE FOLLOWING MINIMUM DIMENSIONS: 2" SQUARE (NOMINAL) BY 4' LONG. TWINE SHOULD BE USED TO BIND BALES. THE USE OF WIRE BINDING IS PROHIBITED BECAUSE IT DOES NOT BIODEGRADE READILY.

PLACEMENT:

A SLOPE BARRIER SHOULD BE USED AT THE TOE OF A SLOPE WHEN A DITCH DOES NOT EXIST. THE SLOPE BARRIER SHOULD BE PLACED ON NEARLY LEVEL GROUND 5' TO 10' AWAY FROM THE TOE OF A SLOPE. THE BARRIER IS PLACED AWAY FROM THE TOE OF THE SLOPE TO PROVIDE ADEQUATE STORAGE FOR SETTLING OUT SEDIMENT. WHEN PRACTICABLE, BALE SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. BALE SLOPE BARRIERS CAN ALSO BE PLACED ALONG RIGHT-OF-WAY FENCE LINES TO KEEP SEDIMENT FROM CROSSING ONTO ADJACENT PROPERTY. WHEN PLACED IN THIS MANNER, THE SLOPE BARRIER WILL NOT LIKELY FOLLOW CONTOURS.

PROPER INSTALLATION METHOD:

EXCAVATE A TRENCH THE LENGTH OF THE PLANNED SLOPE BARRIER THAT IS 4" DEEP AND A BALE'S WIDTH WIDE. MAKE SURE THAT THE TRENCH IS EXCAVATED ALONG A SINGLE CONTOUR. WHEN PRACTICABLE, SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. PLACE THE SOIL ON THE UPSLOPE SIDE OF THE TRENCH FOR LATER USE. PLACE THE BALES IN THE TRENCH, MAKING SURE THAT THEY ARE BUTTED TIGHTLY. TWO STAKES SHOULD BE DRIVEN THROUGH EACH BALE ALONG THE CENTERLINE OF THE DITCH CHECK, APPROXIMATELY 6" TO 8" IN FROM THE BALE ENDS. STAKES SHOULD BE DRIVEN AT LEAST 12" INTO THE GROUND. ONCE ALL THE BALES HAVE BEEN INSTALLED AND ANCHORED, PLACE THE EXCAVATED SOIL AGAINST THE UPSLOPE SIDE OF THE CHECK AND COMPACT IT. THE COMPACTED SOIL SHOULD BE NO MORE THAN 3" TO 4" DEEP.

LIST OF COMMON PLACEMENT/INSTALLATION MISTAKES TO AVOID:

WHEN PRACTICAL, DO NOT PLACE BALE SLOPE BARRIERS ACROSS CONTOURS. SLOPE BARRIERS SHOULD BE PLACED ALONG CONTOURS TO AVOID A CONCENTRATION OF FLOW. CONCENTRATED FLOW OVER A SLOPE BARRIER CREATES A SCOUR HOLE ON THE DOWNSLOPE SIDE OF THE BARRIER. THE SCOUR HOLE EVENTUALLY UNDERMINES THE BALES AND THE BARRIER FAILS. DO NOT PLACE BALE SLOPE BARRIERS IN AREAS WITH SHALLOW SOILS UNDERLAIN BY ROCK. IF THE BARRIER IS NOT ANCHORED SUFFICIENTLY, IT WILL WASH OUT. BALE SLOPE BARRIERS MUST BE DUG INTO THE GROUND. BALES AT GROUND LEVEL DO NOT WORK BECAUSE THEY ALLOW WATER TO FLOW UNDER THE BARRIER.

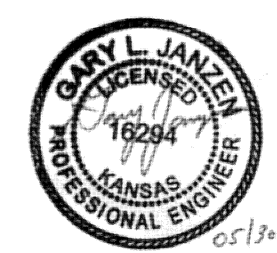
INSPECTION AND MAINTENANCE:

BALE SLOPE BARRIERS SHOULD BE INSPECTED EVERY 7 DAYS AND WITHIN 24 HOURS OF A RAINFALL OF 1/2" OR MORE. THE FOLLOWING IS A LIST OF QUESTIONS THAT SHOULD BE ADDRESSED DURING EACH INSPECTION:

- ARE THERE ANY POINTS ALONG THE SLOPE BARRIER WHERE WATER IS CONCENTRATING?
- DOES WATER FLOW UNDER THE SLOPE BARRIER?
- DOES WATER FLOW THROUGH SPACES BETWEEN ABUTTING BALES?
- ARE ANY BALES DISLODGED?
- ARE BALES DECOMPOSING DUE TO AGE AND/OR WATER DAMAGE?
- DOES SEDIMENT NEED TO BE REMOVED FROM BEHIND THE SLOPE BARRIER?

REVISION DATE: MAY 2013

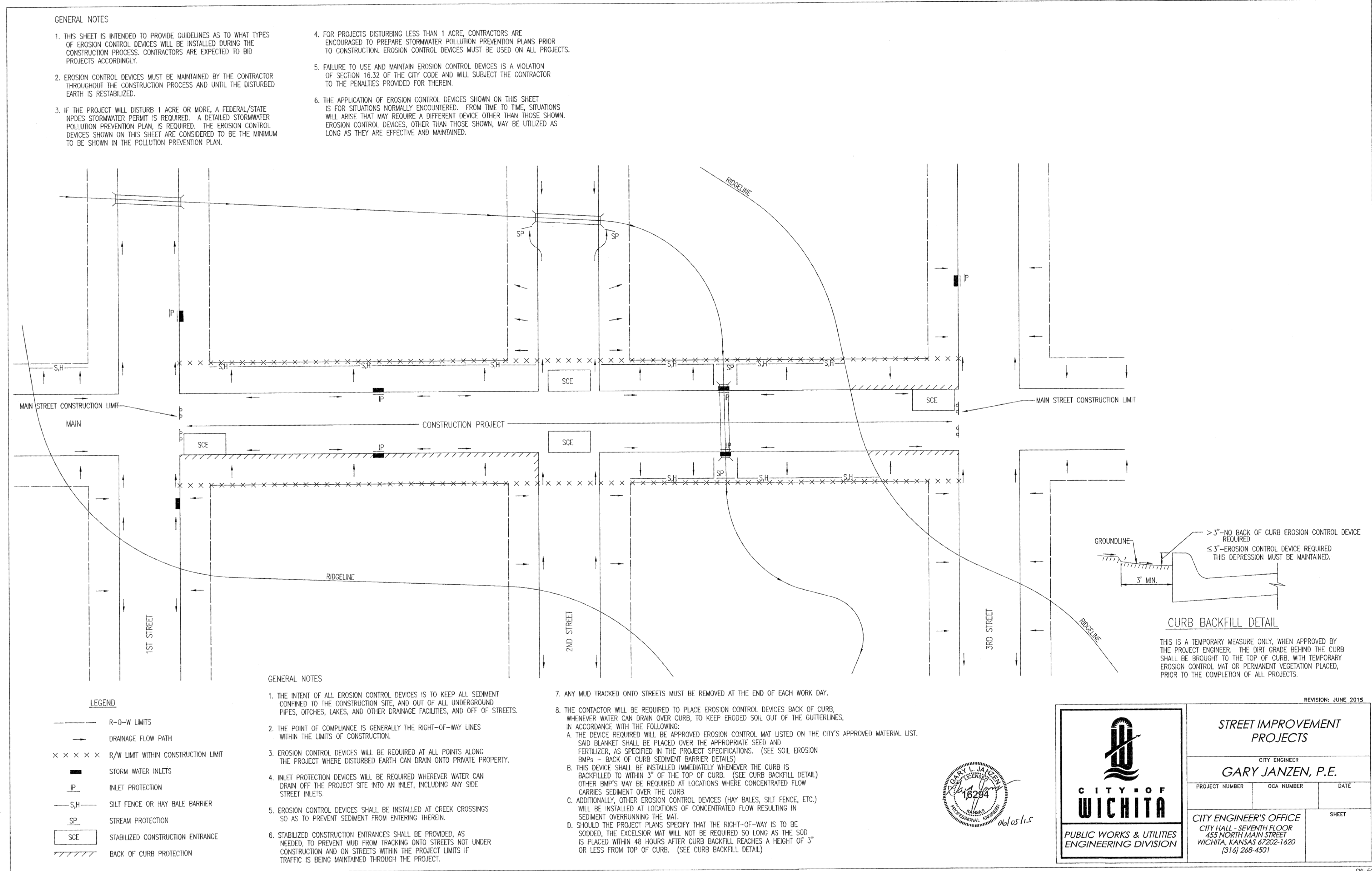
<p>CITY OF WICHITA PUBLIC WORKS & UTILITIES ENGINEERING DIVISION</p>	<p>STRAW BALE DITCH CHECK AND BARRIER DETAILS</p>	
	<p>CITY ENGINEER GARY JANZEN, P.E.</p>	
PROJECT NUMBER	OCA NUMBER	DATE
<p>CITY ENGINEER'S OFFICE CITY HALL - SEVENTH FLOOR 455 NORTH MAIN STREET WICHITA, KANSAS 67202-1620 (316) 268-4501</p>		SHEET



NO.	BY	DATE	REVISION
6.	JAR	07/30/15	ISSUED FOR CONSTRUCTION
5.	JAR	07/16/15	PER REVISED CABING PIPE
4.	JAR	07/15/15	PER CITY COMMENTS
3.	JAR	07/08/15	PER CITY COMMENTS
2.	JAR	06/15/15	PER CITY COMMENTS
1.	JAR	06/03/15	ORIGINAL SUBMITTAL

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KANSAS CITY, KANSAS 66103

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GENERAL NOTES

1. THIS SHEET IS INTENDED TO PROVIDE GUIDELINES AS TO WHAT TYPES OF EROSION CONTROL DEVICES WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS. CONTRACTORS ARE EXPECTED TO BID PROJECTS ACCORDINGLY.
2. EROSION CONTROL DEVICES MUST BE MAINTAINED BY THE CONTRACTOR THROUGHOUT THE CONSTRUCTION PROCESS AND UNTIL THE DISTURBED EARTH IS RESTABILIZED.
3. IF THE PROJECT WILL DISTURB 1 ACRE OR MORE, A FEDERAL/STATE NPDES STORMWATER PERMIT IS REQUIRED. A DETAILED STORMWATER POLLUTION PREVENTION PLAN, IS REQUIRED. THE EROSION CONTROL DEVICES SHOWN ON THIS SHEET ARE CONSIDERED TO BE THE MINIMUM TO BE SHOWN IN THE POLLUTION PREVENTION PLAN.
4. FOR PROJECTS DISTURBING LESS THAN 1 ACRE, CONTRACTORS ARE ENCOURAGED TO PREPARE STORMWATER POLLUTION PREVENTION PLANS PRIOR TO CONSTRUCTION. EROSION CONTROL DEVICES MUST BE USED ON ALL PROJECTS.
5. FAILURE TO USE AND MAINTAIN EROSION CONTROL DEVICES IS A VIOLATION OF SECTION 16.32 OF THE CITY CODE AND WILL SUBJECT THE CONTRACTOR TO THE PENALTIES PROVIDED FOR THEREIN.
6. THE APPLICATION OF EROSION CONTROL DEVICES SHOWN ON THIS SHEET IS FOR SITUATIONS NORMALLY ENCOUNTERED. FROM TIME TO TIME, SITUATIONS WILL ARISE THAT MAY REQUIRE A DIFFERENT DEVICE OTHER THAN THOSE SHOWN. EROSION CONTROL DEVICES, OTHER THAN THOSE SHOWN, MAY BE UTILIZED AS LONG AS THEY ARE EFFECTIVE AND MAINTAINED.


GENERAL NOTES

1. THE INTENT OF ALL EROSION CONTROL DEVICES IS TO KEEP ALL SEDIMENT CONFINED TO THE CONSTRUCTION SITE, AND OUT OF ALL UNDERGROUND PIPES, DITCHES, LAKES, AND OTHER DRAINAGE FACILITIES, AND OFF OF STREETS.
2. THE POINT OF COMPLIANCE IS GENERALLY THE RIGHT-OF-WAY LINES WITHIN THE LIMITS OF CONSTRUCTION.
3. EROSION CONTROL DEVICES WILL BE REQUIRED AT ALL POINTS ALONG THE PROJECT WHERE DISTURBED EARTH CAN DRAIN ONTO PRIVATE PROPERTY.
4. INLET PROTECTION DEVICES WILL BE REQUIRED WHEREVER WATER CAN DRAIN OFF THE PROJECT SITE INTO AN INLET, INCLUDING ANY SIDE STREET INLETS.
5. EROSION CONTROL DEVICES SHALL BE INSTALLED AT CREEK CROSSINGS SO AS TO PREVENT SEDIMENT FROM ENTERING THEREIN.
6. STABILIZED CONSTRUCTION ENTRANCES SHALL BE PROVIDED, AS NEEDED, TO PREVENT MUD FROM TRACKING ONTO STREETS NOT UNDER CONSTRUCTION AND ON STREETS WITHIN THE PROJECT LIMITS IF TRAFFIC IS BEING MAINTAINED THROUGH THE PROJECT.
7. ANY MUD TRACKED ONTO STREETS MUST BE REMOVED AT THE END OF EACH WORK DAY.
8. THE CONTRACTOR WILL BE REQUIRED TO PLACE EROSION CONTROL DEVICES BACK OF CURB, WHENEVER WATER CAN DRAIN OVER CURB, TO KEEP ERODED SOIL OUT OF THE GUTTERLINES, IN ACCORDANCE WITH THE FOLLOWING:
 - A. THE DEVICE REQUIRED WILL BE APPROVED EROSION CONTROL MAT LISTED ON THE CITY'S APPROVED MATERIAL LIST. SAID BLANKET SHALL BE PLACED OVER THE APPROPRIATE SEED AND FERTILIZER, AS SPECIFIED IN THE PROJECT SPECIFICATIONS. (SEE SOIL EROSION BMPs - BACK OF CURB SEDIMENT BARRIER DETAILS)
 - B. THIS DEVICE SHALL BE INSTALLED IMMEDIATELY WHENEVER THE CURB IS BACKFILLED TO WITHIN 3" OF THE TOP OF CURB. (SEE CURB BACKFILL DETAIL)
 - C. ADDITIONALLY, OTHER EROSION CONTROL DEVICES (HAY BALES, SILT FENCE, ETC.) WILL BE INSTALLED AT LOCATIONS OF CONCENTRATED FLOW RESULTING IN SEDIMENT OVERRUNNING THE MAT.
 - D. SHOULD THE PROJECT PLANS SPECIFY THAT THE RIGHT-OF-WAY IS TO BE SODDED, THE EXCELSIOR MAT WILL NOT BE REQUIRED SO LONG AS THE SOD IS PLACED WITHIN 48 HOURS AFTER CURB BACKFILL REACHES A HEIGHT OF 3" OR LESS FROM TOP OF CURB. (SEE CURB BACKFILL DETAIL)

LEGEND

- R-O-W LIMITS
- DRAINAGE FLOW PATH
- x x x x x R/W LIMIT WITHIN CONSTRUCTION LIMIT
- STORM WATER INLETS
- IP INLET PROTECTION
- S.H. SILT FENCE OR HAY BALE BARRIER
- SP STREAM PROTECTION
- SCE STABILIZED CONSTRUCTION ENTRANCE
- /// BACK OF CURB PROTECTION

REVISION: JUNE 2015



CITY OF WICHITA
PUBLIC WORKS & UTILITIES
ENGINEERING DIVISION

STREET IMPROVEMENT PROJECTS

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER	OCA NUMBER	DATE

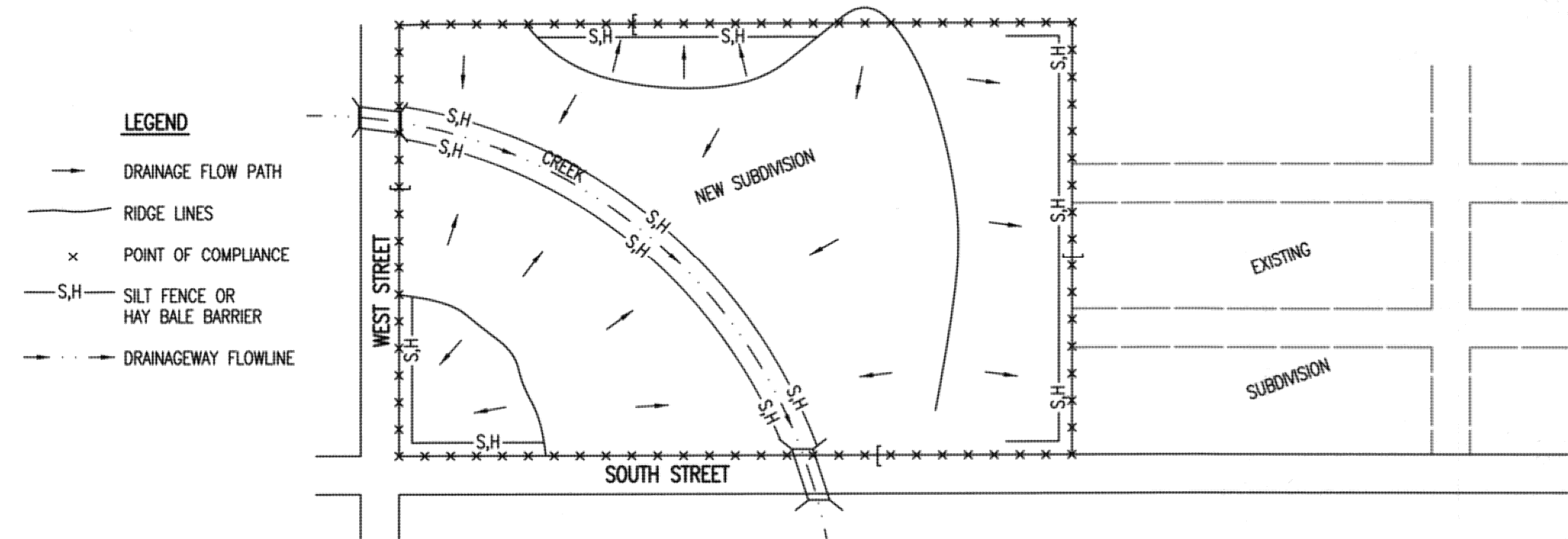
CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

NO.	BY	DATE
6.	JAR	RLC 07/30/15
5.	JAR	RLC 07/16/15
4.	JAR	RLC 07/15/15
3.	JAR	FHC 07/08/15
2.	JAR	FHC 06/18/15
1.	JAR	FHC 06/03/15
	JAR	FHC 05/15/15

ISSUED FOR CONSTRUCTION	6. JAR	RLC	07/30/15
PER REVISED CASING PIPE	5. JAR	RLC	07/16/15
PER CITY COMMENTS	4. JAR	RLC	07/15/15
PER CITY COMMENTS	3. JAR	FHC	07/08/15
PER CITY COMMENTS	2. JAR	FHC	06/18/15
PER CITY COMMENTS	1. JAR	FHC	06/03/15
ORIGINAL SUBMITTAL	JAR	FHC	05/15/15
REVISION	NO.	BY	DATE

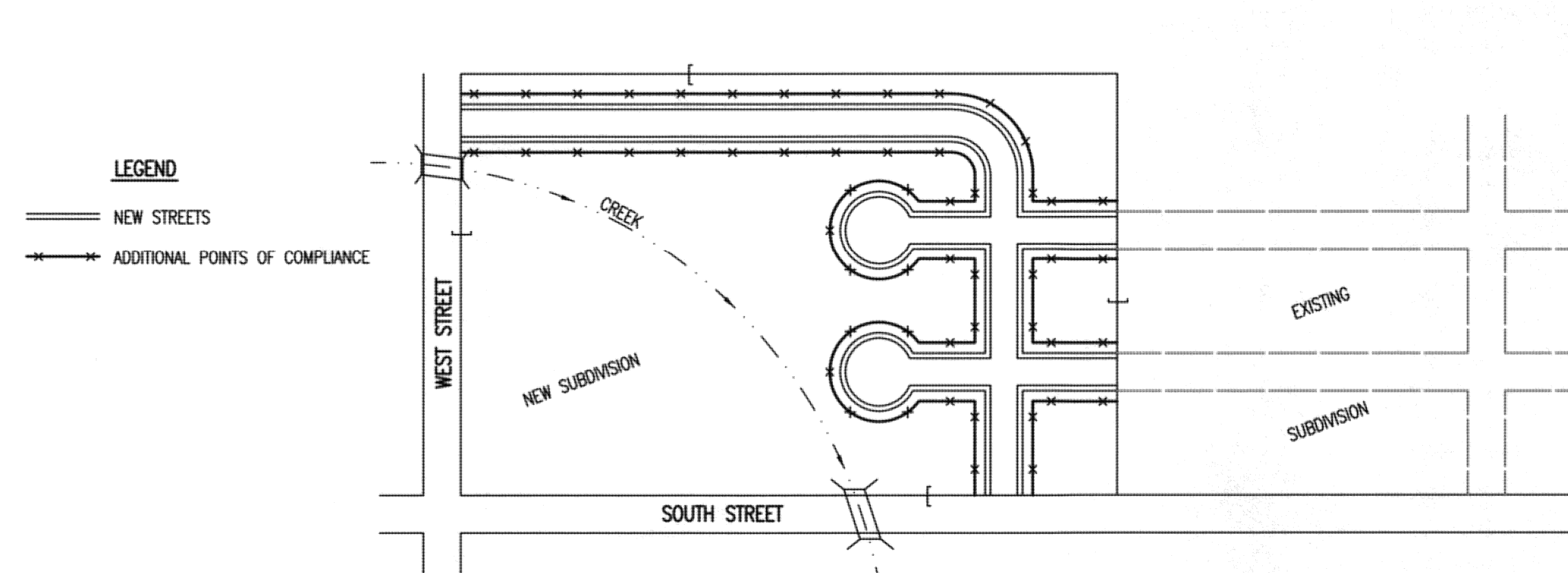
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PHASE 1 - INITIAL EARTHWORK AND UTILITIES (EXCEPT STORM SEWER)



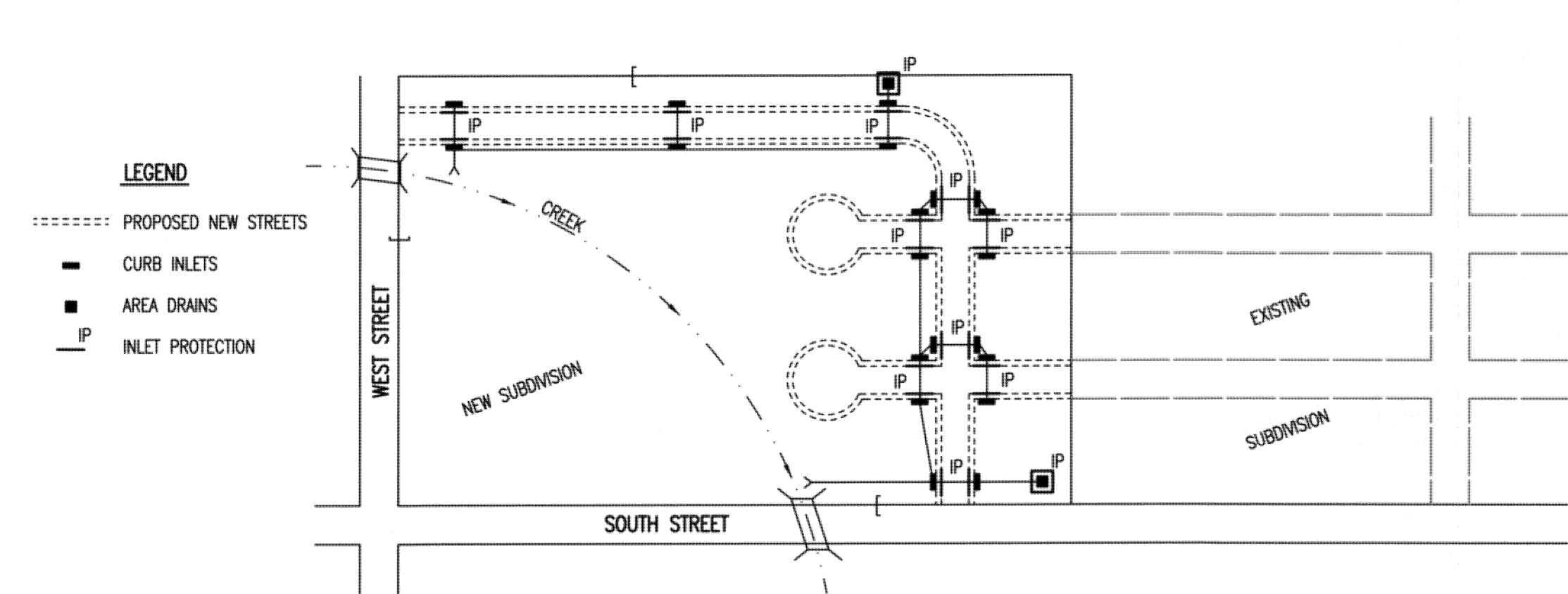
- DURING THIS PHASE OF SUBDIVISION CONSTRUCTION, THE POINTS OF COMPLIANCE ARE THE PERIMETER BOUNDARIES AND ANY DRAINAGE WAYS OR STORM SEWERS DRAINING THROUGH OR FROM THE SITE. SHOULD LAKES BE CONSTRUCTED WITHIN THE SUBDIVISION THAT WILL DISCHARGE DURING STORMS, THEY ARE ALSO A POINT OF COMPLIANCE.
- HAY BALES OR SILT FENCE MUST BE CONSTRUCTED ALONG THE PROPERTY LINE WHERE ON SITE WATER CAN DRAIN OFF THE PROPERTY. THESE EROSION CONTROL DEVICES WILL ALSO BE INSTALLED ALONG ANY DRAINAGE DITCH OR LAKE THAT CAN DISCHARGE.
- SHOULD SILT OR SEDIMENT ENTER THE DITCHES OR STREETS ON THE ADJACENT BOUNDARY STREETS, APPROPRIATE EROSION CONTROL DEVICES WILL BE PLACED WITHIN THE SUBDIVISION TO PREVENT THIS.
- ANY MUD TRACKED ONTO ADJACENT STREETS WILL BE REMOVED WITHIN 48 HOURS OR BY FRIDAY AT 6:00 PM, WHICHEVER IS EARLIER.
- CONTRACTORS WORKING WITHIN THE SITE WILL NOT BE REQUIRED TO USE INDIVIDUAL EROSION CONTROL DEVICES AS LONG AS THOSE SPECIFIED ABOVE ARE IN PLACE AND EFFECTIVE. CONTRACTORS WORKING ON THE BOUNDARY LINE STREETS OR ON ADJACENT PROPERTIES TO EXTEND UTILITIES ARE EXPECTED TO USE EROSION CONTROL DEVICES AT THEIR WORK LOCATIONS, AS NEEDED.
- UTILIZE STABILIZED CONSTRUCTION ENTRANCE AT ENTRANCE AND EXIT ONTO ANY EXISTING PUBLIC STREETS.
- IF THE INITIAL EARTH WORK AND UTILITIES ARE DONE AS PART OF A PUBLIC IMPROVEMENT PROJECT, THESE EROSION CONTROL DEVICES WILL BE INSTALLED BY THE CONTRACTOR AS SPECIFIED IN THE INDIVIDUAL PROJECT CONTRACTS. THE CONTRACTOR WILL MAINTAIN THE DEVICES UNTIL COMPLETION OF THE CONTRACT, AT WHICH TIME THE DEVELOPER WILL ASSUME MAINTENANCE RESPONSIBILITIES. IF THESE CONTRACTS ARE NOT PUBLIC IMPROVEMENT PROJECTS, THE DEVELOPER WILL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THESE DEVICES.
- WITHIN 14 DAYS OF COMPLETION OF EARTHWORK ACTIVITIES IN ANY GIVEN AREA, THAT AREA SHALL BE TEMPORARILY OR PERMANENTLY SEEDED AND MULCHED.

PHASE 3 - STREET CONSTRUCTION



- DURING THIS PHASE OF SUBDIVISION CONSTRUCTION, NEW STREETS ARE INSTALLED. ALL EROSION CONTROL DEVICES INSTALLED DURING PHASE 1 AND 2 MUST STILL BE MAINTAINED. THE POINT OF COMPLIANCE NOW SHIFTS TO THE BACK OF CURB ALONG EACH STREET.
- CURB OPENING INLET PROTECTION:
 - SUMP AREAS - INLET PROTECTION SHALL BE PROVIDED WHEN STREET SUBGRADE WORK IS COMPLETED.
 - NON-SUMP LOCATIONS - PROVIDE INLET PROTECTION AS SOON AS BASE COURSE ASPHALT IS INSTALLED, BEFORE THE SURFACE COURSE LIFT.
- EROSION CONTROL DEVICES WILL BE REQUIRED BACK OF CURB WHEREVER WATER CAN FLOW OVER THE CURB AND THE CURB HAS BEEN BACKFILLED TO WITHIN 3" OR LESS OF THE TOP OF CURB (SEE CURB BACKFILL DETAIL). FOR CURBS NOT YET ENTIRELY BACKFILLED (3" OR MORE BELOW TOP OF CURB), ADDITIONAL DEVICES WILL BE REQUIRED AT POINTS WHERE WATER BREAKS OVER CURB WHICH COULD RESULT IN THE PLACEMENT OF SEDIMENT IN THE GUTTER.
- SEE DETAIL SHEET FOR BACK OF CURB PROTECTION.
- THE BACK OF CURB PROTECTION SPECIFIED ON THIS PLAN MAY HAVE TO BE SUPPLEMENTED WITH HAY BALE OR SILT FENCE EROSION CONTROL DEVICES AT LOCATIONS WHERE CONCENTRATED FLOW RESULTS IN SEDIMENT BEING CARRIED OVER THE EXCESSIOR MATS.
- THE STREET CONTRACTOR WILL BE RESPONSIBLE FOR INSTALLING BACK OF CURB EROSION CONTROL DEVICES.
- THE INDIVIDUAL LOT OWNERS WILL BE RESPONSIBLE FOR MAINTAINING THE BACK OF CURB EROSION CONTROL DEVICES IN FRONT OF THEIR LOTS UNTIL SUCH TIME AS ADJACENT DISTURBED EARTH IS STABILIZED WITH GRASS OR SOD.

PHASE 2 - INSTALLATION OF STORM SEWER

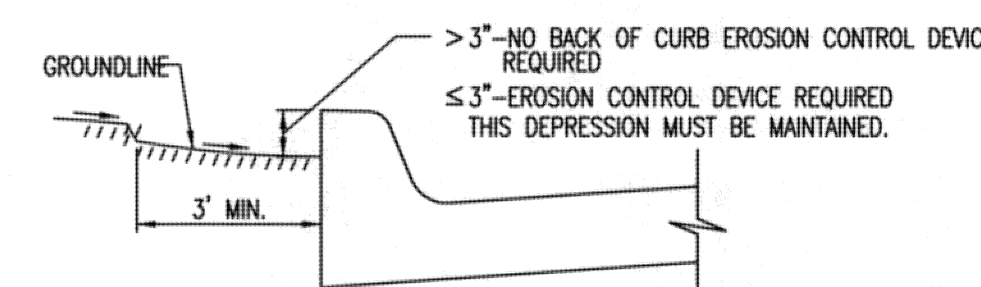


- DURING THIS PHASE OF SUBDIVISION DEVELOPMENT, ALL EROSION CONTROL DEVICES REQUIRED IN PHASE 1 SHALL REMAIN IN PLACE AND BE MAINTAINED.
- AS NEW STORM SEWERS, WITH INLETS, ARE INSTALLED, THE STORM SEWERS MUST NOW BE PROTECTED SO ALL NEW INLETS BECOME POINTS OF COMPLIANCE.
- AREA DRAINS - AS SOON AS WATER CAN FLOW INTO THESE DRAINS, HAY BALE OR SILT FENCE PROTECTION WILL BE INSTALLED AROUND THEM.
- CURB OPENING INLETS - AS SOON AS WATER CAN FLOW INTO THESE DRAINS, INLET PROTECTION DEVICES MUST BE INSTALLED. IF WATER CANNOT FLOW INTO CURB INLETS UNTIL STREET CONSTRUCTION IS COMPLETE, THEN STREET CONTRACTOR WILL INSTALL INLET PROTECTION. SEE PHASE 3 - STREET CONSTRUCTION.
- THE STORM SEWER CONTRACTOR WILL BE RESPONSIBLE FOR INSTALLING THESE DEVICES.
- THE SUBDIVISION DEVELOPER WILL MAINTAIN THESE EROSION CONTROL DEVICES ONCE INSTALLED.
- ALL DISTURBED GROUND WILL BE FINAL GRADED AND TEMPORARILY OR PERMANENTLY SEEDED WITHIN 14 DAYS IF COMPLETION OF WORK IN ANY GIVEN PART OF THE SUBDIVISION.
- ONCE ALL DISTURBED GROUND DRAINING TO AN INLET HAS BEEN RESTABILIZED WITH GRASS OR SOD, THE SUBDIVISION DEVELOPER WILL BE RESPONSIBLE FOR PERMANENTLY REMOVING THE INLET PROTECTION.

GENERAL NOTES

- THE INTENT OF ALL EROSION CONTROL DEVICES IS TO PREVENT ERODED SOIL FROM ENTERING DITCHES, STORM SEWERS, LAKES, STREETS OR ANY OTHER DRAINAGE FEATURE.
- THIS SHEET IS INTENDED TO PROVIDE GUIDELINES AS TO WHAT TYPE OF EROSION CONTROL DEVICES WILL BE INSTALLED DURING THE CONSTRUCTION PROCESS. CONTRACTORS ARE EXPECTED TO BID PROJECTS ACCORDINGLY.
- EROSION CONTROL DEVICES SHALL BE MAINTAINED DURING THE CONSTRUCTION PROCESS TO REMAIN EFFECTIVE. MAINTENANCE SHALL BE AS INDICATED ON SOIL EROSION BMP'S DETAIL SHEETS.
- PERSONS DESTROYING EROSION CONTROL DEVICES SHALL BE RESPONSIBLE FOR IMMEDIATELY REPAIRING THEM OR INSTALLING SUITABLE REPLACEMENT DEVICES.
- THE DEVELOPMENT OF ANY SUBDIVISION THAT DISTURBS 1 ACRE OR MORE WILL REQUIRE A FEDERAL/STATE NPDES STORMWATER PERMIT. THE PREPARATION OF A STORMWATER POLLUTION PREVENTION PLAN IS REQUIRED. EROSION CONTROL DEVICES ARE REQUIRED. THE DETAILS SHOWN ON THIS SHEET ARE THE MINIMUM STANDARDS TO BE SHOWN ON POLLUTION PREVENTION PLANS.
- FOR SUBDIVISIONS SMALLER THAN 1 ACRE, SOIL EROSION DEVICES ARE REQUIRED. ALSO, DEVELOPERS AND CONTRACTORS ARE ENCOURAGED TO DEVELOP POLLUTION PREVENTION PLANS FOR EACH PROJECT PRIOR TO CONSTRUCTION.
- FAILURE TO USE AND MAINTAIN SOIL EROSION DEVICES IS A VIOLATION OF SECTION 16.32 OF THE CITY CODE AND WILL SUBJECT THE SUBDIVISION DEVELOPER AND CONTRACTORS TO THE PENALTIES PROVIDED THEREIN.
- THE APPLICATION OF EROSION CONTROL DEVICES SHOWN ON THIS SHEET IS FOR SITUATIONS NORMALLY ENCOUNTERED. FROM TIME TO TIME, SITUATIONS WILL ARISE THAT MAY REQUIRE DEVICES OTHER THAN THAT SHOWN. EROSION CONTROL DEVICES, OTHER THAN THOSE SHOWN, MAY BE UTILIZED SO LONG AS THEY ARE EFFECTIVE AND MAINTAINED.
- A STABILIZED EARTH SURFACE IS DEFINED AS ONE THAT IS HARD SURFACED WITH CONCRETE, ASPHALT, OR THE LIKE, OR ONE ON WHICH 70% OF THE GRASS HAS GERMINATED ON THE ENTIRE SURFACE.

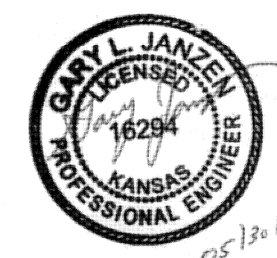
SEE DETAIL SHEET FOR BACK OF CURB PROTECTION DETAIL



CURB BACKFILL DETAIL (STREET CONSTRUCTION ONLY)

THIS IS A TEMPORARY MEASURE ONLY, WHEN APPROVED BY THE PROJECT ENGINEER. THE DIRTY GRADE BEHIND THE CURB SHALL BE BROUGHT TO THE TOP OF CURB, WITH TEMPORARY EROSION CONTROL MAT OR PERMANENT VEGETATION PLACED, PRIOR TO THE COMPLETION OF ALL PROJECTS.

REVISION DATE: MAY 2013



SUBDIVISION DEVELOPMENT PROCESS

CITY ENGINEER
GARY JANZEN, P.E.

PROJECT NUMBER OCA NUMBER DATE

CITY ENGINEER'S OFFICE
CITY HALL - SEVENTH FLOOR
455 NORTH MAIN STREET
WICHITA, KANSAS 67202-1620
(316) 268-4501

SHEET

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