

PHASING	HANDLING OF TRAFFIC	MAJOR CONSTRUCTION ITEMS	REMARKS
1	Close the outside lane of eastbound traffic on 17th Street. Maintain two-lanes of westbound and one lane of eastbound traffic on 17th Street.	Construct water line improvements. Remove and replace concrete pavement above water line as shown in the construction drawings.	Refer to the one lane closure typical traffic control plan sheet. Phases 1, 2 and 3 may be constructed in different order or combined as desired by the contractor if approved by the engineer.
2	Close the outside lane of westbound traffic on 17th Street. Maintain two-lanes of eastbound and one lane of westbound traffic on 17th Street.	Construct storm sewer improvements. Remove and replace concrete pavement above storm sewer line as shown in the construction drawings.	Refer to the one lane closure typical traffic control plan sheet. Phases 1, 2 and 3 may be constructed in different order or combined as desired by the contractor if approved by the engineer.
3	Close one side of the roadway on 17th Street and maintain one lane of traffic in each direction in the other two remaining lanes of 17th Street. Multiple set-ups will be required. The work zone length for each traffic control set-up shall be field determined and approved by the engineer.	Construct the various pavement repairs as shown in the construction drawings.	Refer to the pavement rehab traffic control plan sheet. Phases 1, 2 and 3 may be constructed in different order or combined as desired by the contractor if approved by the engineer.
4	Allow full-use of 17th Street between Hillside and Oliver. Provide temporary lane closures as necessary to complete the construction items to occur this phase.	Construct edge grinding, asphalt overlay and pavement marking. Perform all site restoration activities and all other remaining work items that have yet to be constructed.	Temporary lane closures and other traffic control operations used in this phase shall follow appropriate standard traffic control standards.

All signs and pavement markings conflicting with construction traffic control shall be covered or removed as directed by the Engineer.

As the various construction activities progress, certain situations may arise that will preclude adhering to the original construction sequence or which, in the opinion of the Contractor, would readily adapt themselves to a more efficient phasing operation. Should this occur, the Contractor may submit to the Engineer an alternative plan for approval.

All traffic control signs and devices shall be in conformance with the latest edition of the Manual on Uniform Traffic Control Devices.

Contractor shall provide temporary asphalt pavement where necessary to bridge grade differentials where 17th Street pavement has been removed in front of a property requiring access. Cost of Temporary Surfacing shall be paid for as "AC Pavement 6" Temporary". The plan quantity shown is an arbitrary amount. The actual quantity will be determined in the field during construction as approved by the Engineer.

**NOTES:**  
**BARRICADE PLACEMENT:**

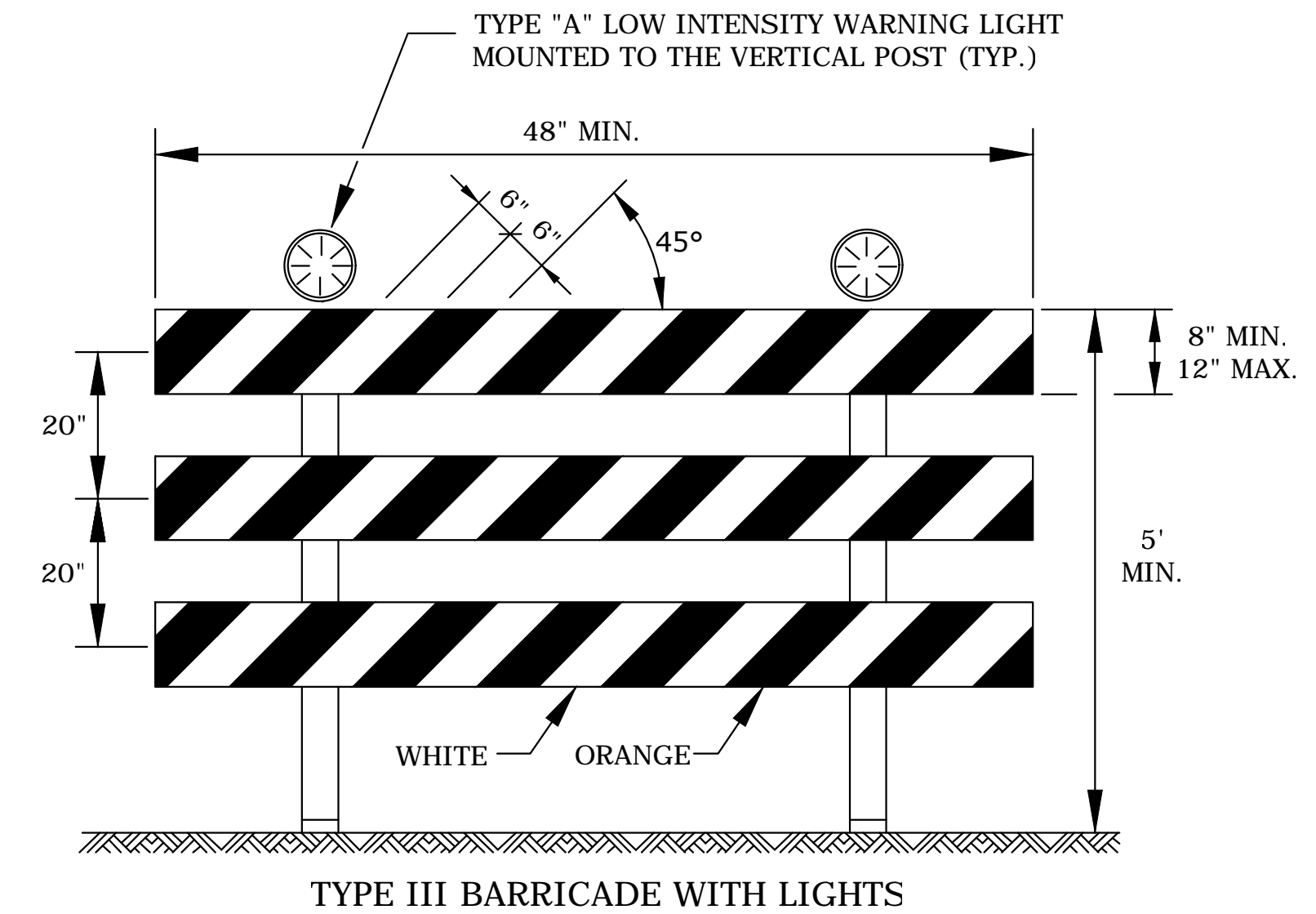
**A) COMPLETE ROAD CLOSURE**

WHEN A ROADWAY IS CLOSED, TYPE III BARRICADES SHALL BE PLACED END-TO-END TO COMPLETELY COVER THE ROADWAY AND SHOULDERS. WHEN ACCESS MUST BE ALLOWED FOR CONSTRUCTION OR OTHER OFFICIAL/GOVERNMENT VEHICLES, TYPE III BARRICADES SHALL BE LONGITUDINALLY STAGGERED FAR ENOUGH APART FROM ONE ANOTHER TO ALLOW SAFE PASSAGE OF VEHICLES AND MAINTAIN THE APPEARANCE OF A CLOSED ROADWAY. TYPE III BARRICADES SHALL BE REALIGNED AND PLACED END-TO-END TO DENY ANY ACCESS WHEN THE CONSTRUCTION ACTIVITY HAS CEASED FOR THE DAY.

**B) ROAD CLOSED - LOCAL TRAFFIC**

AS SHOWN IN FIGURE 4, WHEN LOCAL TRAFFIC MUST BE ALLOWED ACCESS INTO THE WORK ZONE, TYPE III BARRICADES SHALL BE LONGITUDINALLY STAGGERED TO MAINTAIN THE APPEARANCE OF A CLOSED ROADWAY. A SECOND LINE OF END-TO-END TYPE III BARRICADES SHALL BE PLACED JUST BEYOND THE LAST ACCESS POINT IN THE WORK ZONE, TO COMPLETELY CLOSE THE ROADWAY AS DESCRIBED IN NOTE 2-A.

AS SHOWN IN FIGURE 1 AND FIGURE 3, AT THE POINT WHERE THRU TRAFFIC MUST DETOUR AND LOCAL TRAFFIC CAN PROCEED TO THE LOCATION WHERE THE ROADWAY IS COMPLETELY CLOSED, THE R11-3A (ROAD CLOSED # MILES AHEAD LOCAL TRAFFIC ONLY) OR R11-4 (ROAD CLOSED LOCAL TRAFFIC ONLY OR ROAD CLOSED TO THRU TRAFFIC) SIGN SHALL BE USED WITH TYPE III BARRICADES (WINGED POSITION), PLACED ON THE SHOULDERS OF ROADWAY.



**TYPE III BARRICADE WITH LIGHTS**


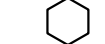



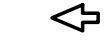
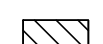



THE ENTIRE AREA OF BARRICADE RAILS, BOTH FRONT AND BACK, SHALL HAVE ASTM TYPE III SHEETING.

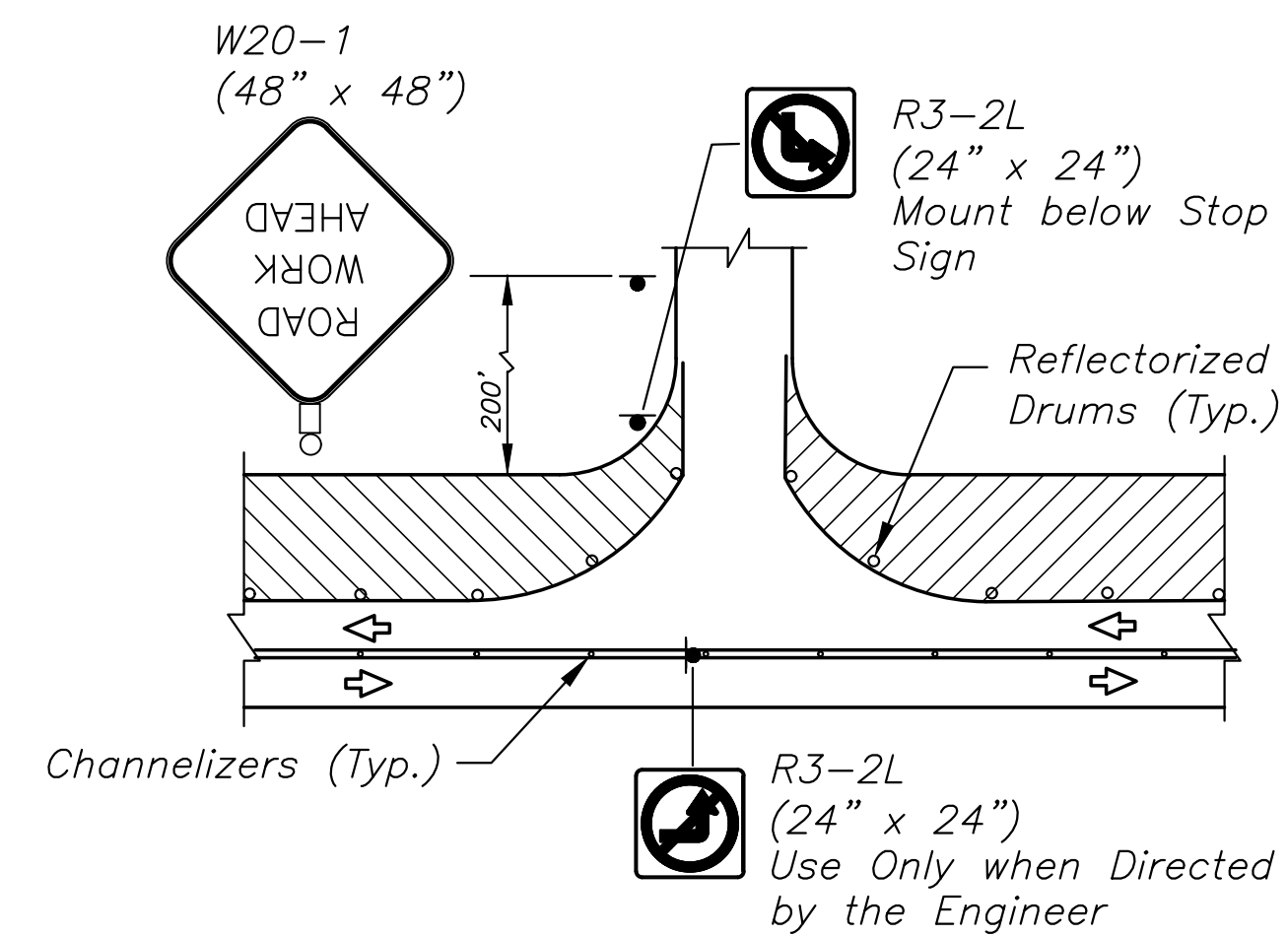
THE STRIPES SHALL SLOPE DOWNWARD TO THE SIDE TRAFFIC IS TO PROCEED OR TOWARD THE CENTER OF THE ROADWAY AT ROAD CLOSURES.

APPROVED SIGNS MOUNTED ON TYPE III BARRICADES SHOULD NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

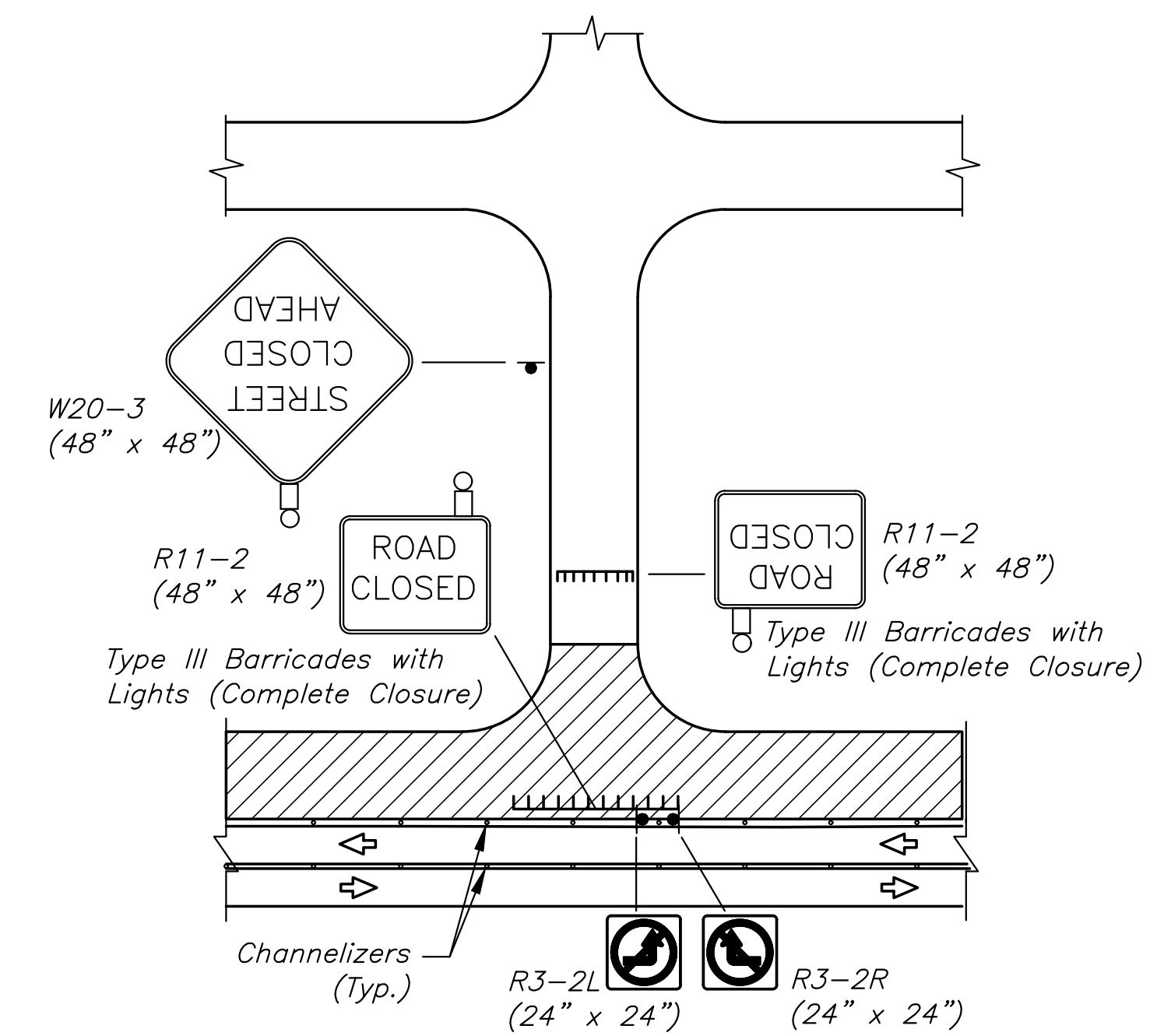
WHEN BARRICADES ARE PLACED END-TO-END OR STAGGERED, A TYPE "A" LOW INTENSITY WARNING LIGHT SHALL BE MOUNTED TO THE VERTICAL POST NEAR EACH OUTSIDE CORNER OF THE END BARRICADES.

**LEGEND**

-  Type III Barricades with lights
-  Mount On Type III Barricade
-  Type B Warning Light
-  One Post Sign
-  Two Post Sign
-  Traffic Flow
-  Work Area
-  Reflectorized Drum
-  Channelizer
-  Mount Overhead on Signal Cross-arm



**TYPICAL ENTRANCE AND SIDE STREET  
(TWO-WAY TRAFFIC ON MAIN ROAD)**



**TYPICAL CLOSED SIDE STREET**

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**CONSTRUCTION SEQUENCE SUMMARY**

PROJECT NO.	472-85215	
DATE	5/3/2017	
SCALE	NONE	
DESIGNED	DRAWN	CHECKED
JRA	WNJ	JRA

NO.	REVISION	DATE

SHEET NO.