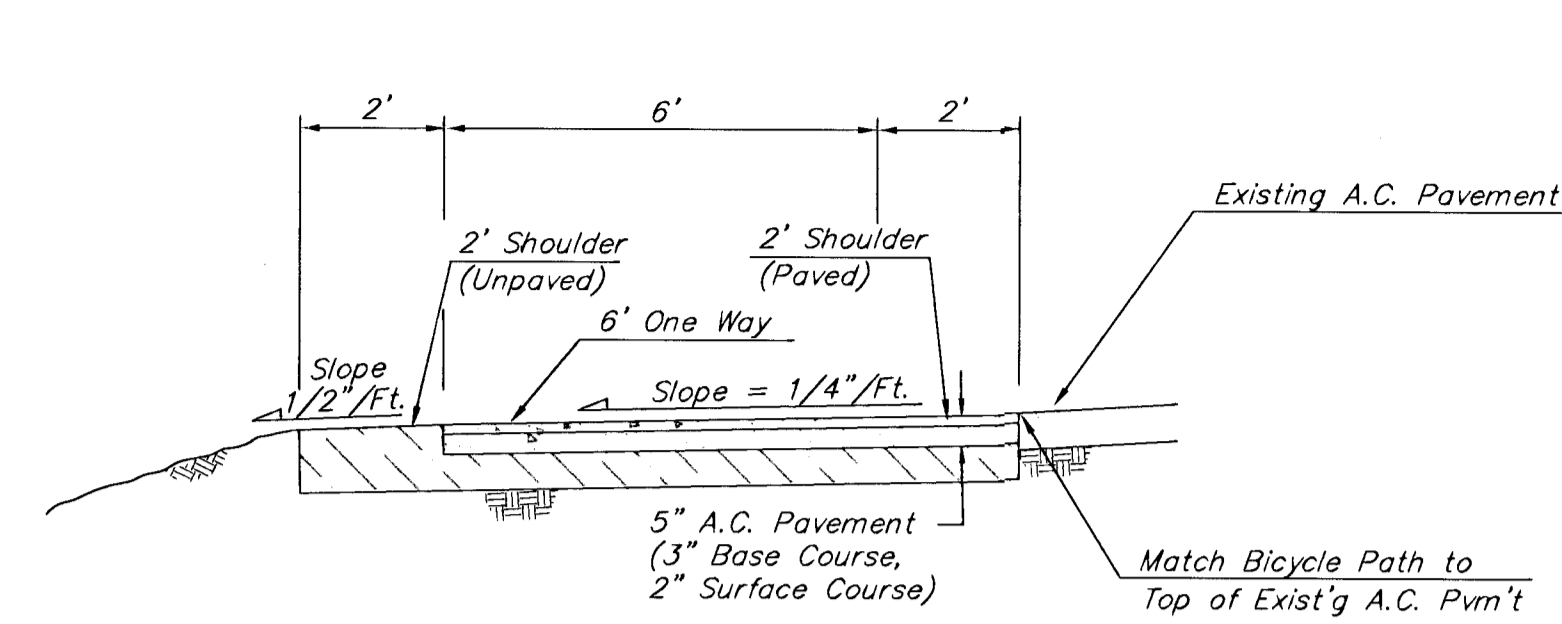


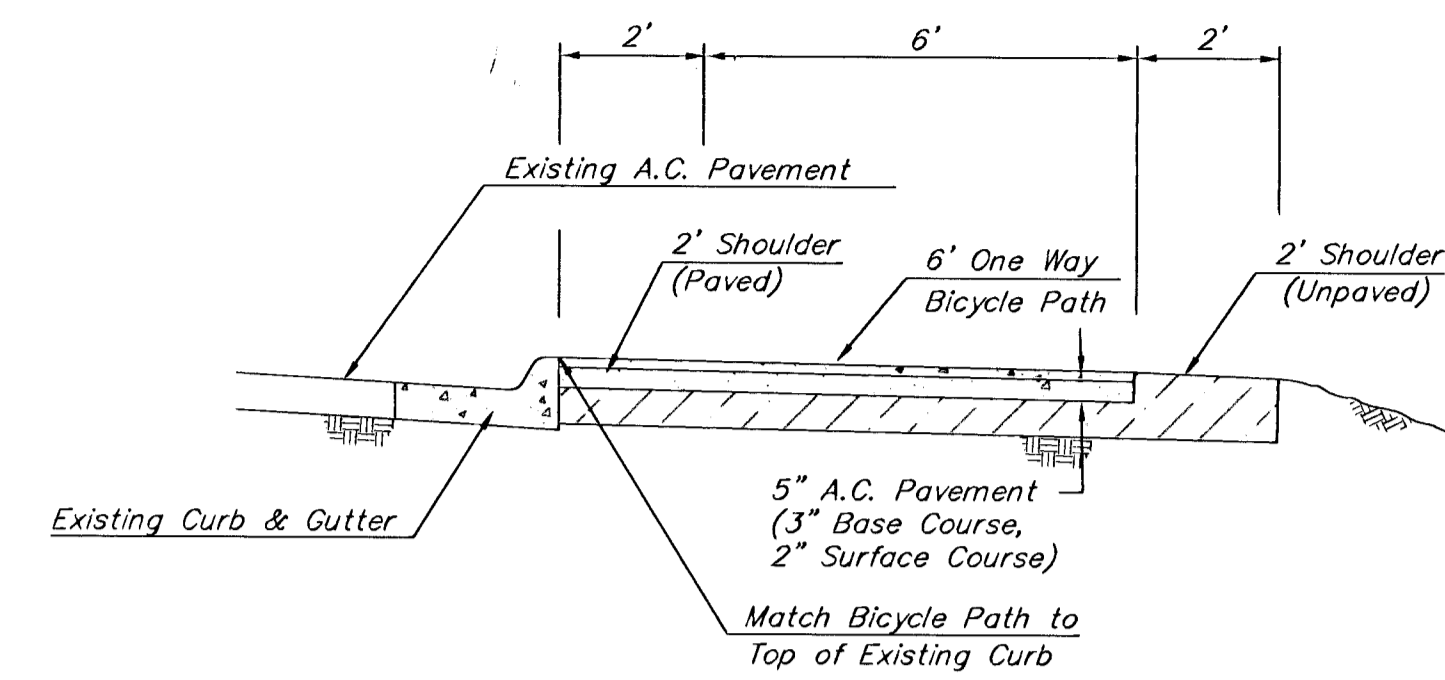
GENERAL NOTES:

- Utility service lines, poles, valve boxes, meters, etcetera are to be adjusted as necessary by others prior to construction unless the plans specifically call for their adjustment by the Contractor or unless the plans specifically identify a utility to be adjusted by its owner during construction. Existing utilities and their location, as shown on the plans, represent the best information obtainable for design. The contractor will be required to work around existing utilities within the right-of-way which do not conflict with proposed construction. Relocations or adjustments to utilities shall be subsidiary to "Right of Way Clearing & Preparation" in bid items.
- Rubble from the removal of miscellaneous structures and excess excavation which is to be wasted shall be disposed of on sites to be provided by the Contractor. These sites shall be approved by the Engineer as to suitability, appearance and site location. Locations that, in the opinion of the Engineer, will leave an unsightly appearance will not be approved. All disposal sites must be approved by the Kansas Department of Health and Environment. Material either stockpiled or disposed of in a flood plain would require a Kansas State Board of Agriculture permit. Any material dumped in waters of the United States or wetlands is subject to U.S. Corps. of Engineers permitting regulations. Any material buried or stockpiled beyond approved construction limits would require additional archaeological investigations unless buried in a previously approved borrow location. Rubble removal and disposal shall be subsidiary to "Right of Way Clearing & Preparation" in bid items.
- All areas disturbed by construction operations shall be seeded with Guyton Bermuda at a rate of 110 lbs. per acre. Planting time shall be between April 15 and June 15. The cost of seeding shall be subsidiary to the bid item "Site Restoration."
- Trees and shrubs in public right-of-way which are in direct conflict with proposed new construction shall be removed by the Contractor with the Engineer's approval. Trees and shrubs which are not in direct conflict with proposed new construction shall be saved and protected from damage. Trees that are to be relocated are noted on the plans. Tree removal and relocation costs are subsidiary to "Right of Way Clearing & Preparation" in bid items.
- The Contractor shall be responsible for preserving property irons. The Contractor will be required to re-establish any property irons which are damaged or destroyed by his construction operations. Such irons shall be re-established by a licensed land surveyor in accordance with state laws. Cost to be subsidiary to "Site Restoration" in bid items.
- Properties within the project limits may have underground sprinkler systems in public right-of-way which conflict with new construction. Contractor will be required to remove such improvements should they not be removed by their owner at the time of construction of the project. The Contractor will be required to salvage all sprinkler heads and/or valves and give such material to their owner. Portions of underground sprinkler systems not in conflict with new construction shall be protected from damage and shall remain in place. All work in connection with underground sprinkler systems shall be considered as subsidiary to "Site Restoration" in bid items.
- Coordinate sign relocations with Traffic Engineering at 268-4666. Material designated to be removed but not relocated shall be salvaged for removal by City forces. Installation of new signs, posts, bolts, etc. and relocation of existing signs shall be subsidiary to "Street Signage" in bid items.
- Fabric base reinforcement shall be a regular grid structure formed by biaxially drawing a continuous sheet of select polypropylene material and shall have aperture geometry and rib and junction cross-sections sufficient to permit significant mechanical interlock with the material being reinforced. The geogrid shall have flexural rigidity and high tensile modulus in relation to the material being reinforced and shall also have high continuity of tensile strength through all ribs and junctions of the grid structure. The geogrid shall maintain its reinforcement and interlock capabilities under repeated dynamic loads while in service and shall also be resistant to ultraviolet degradation, to damage under normal construction practices and to all forms of biological or chemical degradation normally encountered in the material being reinforced. The geogrid shall conform in all respects to the property requirements listed in the City of Wichita supplemental specifications for Fabric for Reinforced Crushed Rock Base, available at the City of Wichita Engineering Dept. Fabric reinforcement shall be installed per manufacturer's recommendations. Fabric base shall be incidental to line item for crushed rock base.

- All work associated with the installation of the wheelchair ramps, including removal of existing curb and gutter and construction of depressed curb and gutter, shall be included in the bid item "Wheelchair Ramp."
- Unless noted otherwise, all work and materials shall conform to the appropriate sections of the 1990 edition "Standard Specifications for State Road and Bridge Construction" by the Kansas Department of Transportation.
- Rock base is to be compacted and smoothed prior to placement of asphalt. Tack coat will not be applied to rock base.
- A tack coat of emulsified asphalt (SC-1H or CSS-1H) shall be applied at an approximate rate of 0.05 gallons per square yard between each lift of asphaltic material shall be subsidiary to the asphalt paving line item.
- Bituminous base and asphaltic concrete wearing surface shall be placed with laydown machine having automatic controls for line and grade.
- Concrete bikepath shall be reinforced with W4xW4 - 4 X 4 wire mesh. Wire mesh shall be subsidiary to the concrete pavement bid item.
- PVC pipe shall be schedule 40.



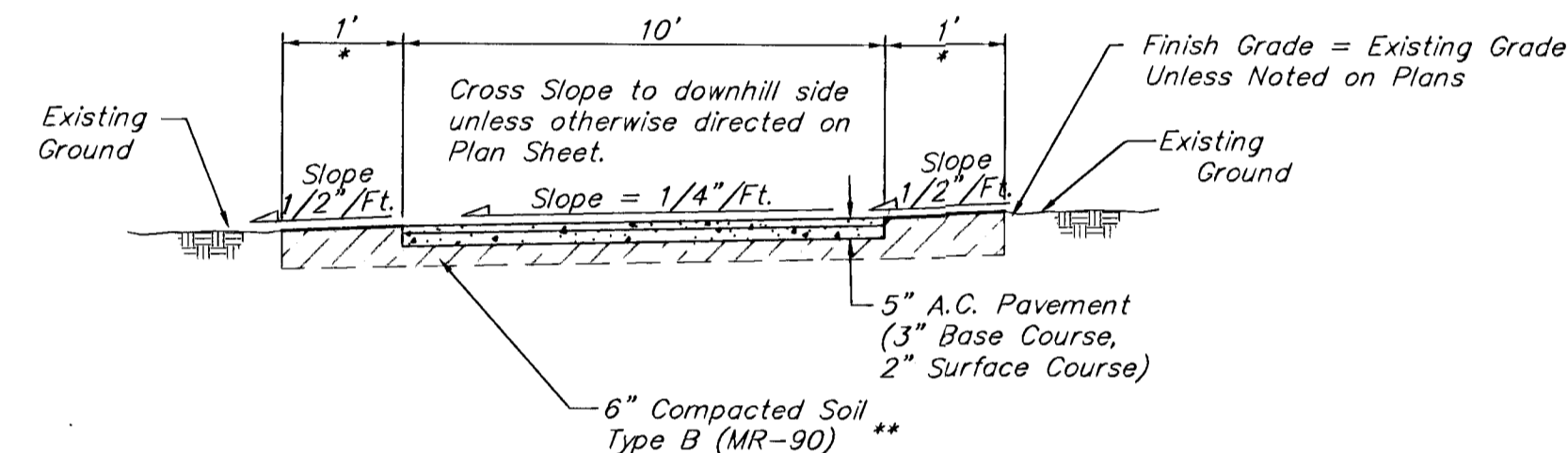
TYPICAL SECTION
ONE WAY PATH ADJACENT TO A.C. PAVEMENT
NO SCALE



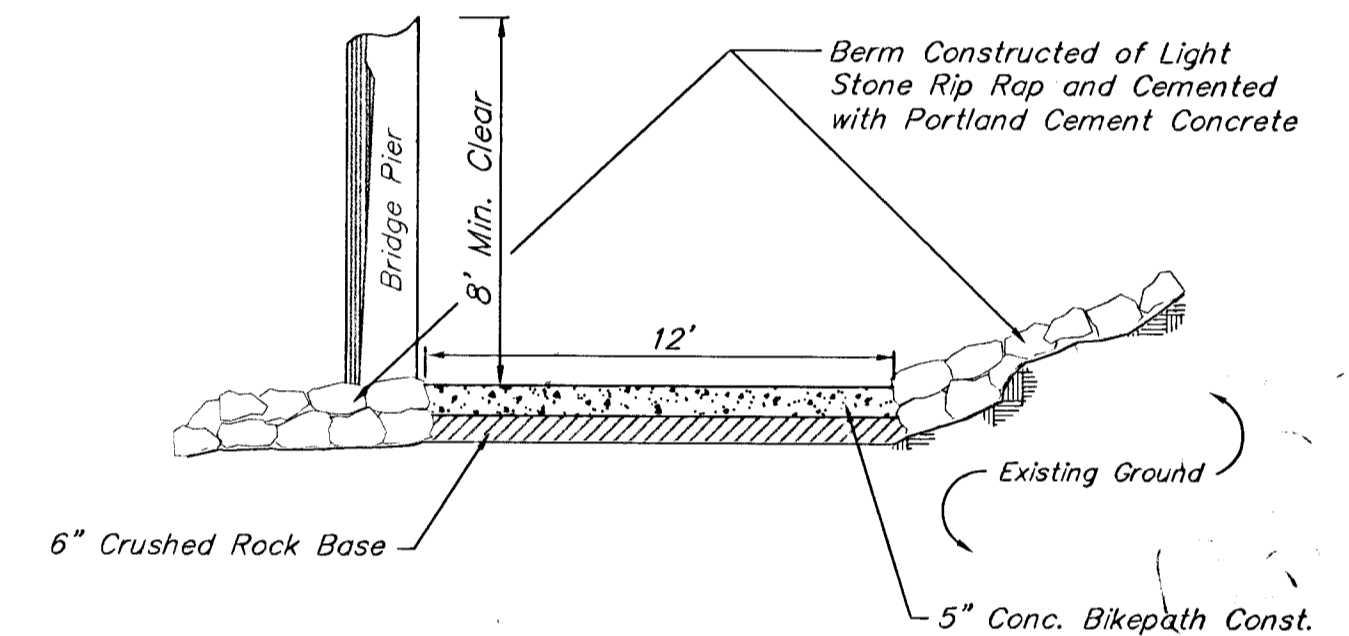
TYPICAL SECTION
ONE WAY PATH ADJACENT TO CURB
NO SCALE

BENCHMARKS:

- City of Wichita Bench Mark Disk in traffic signal base, north side 31st Street South & west line Turnpike Drive Elev. = 165.99
- City of Wichita Bench Mark Disk in traffic signal base, SE intersection, Oliver Street and 31st Street South Elev. = 164.48
- City of Wichita Bench Mark Disk in sidewalk at NE corner of bridge approx. 300± west of KTA & 395± east of centerline McAdam Elev. = 123.02

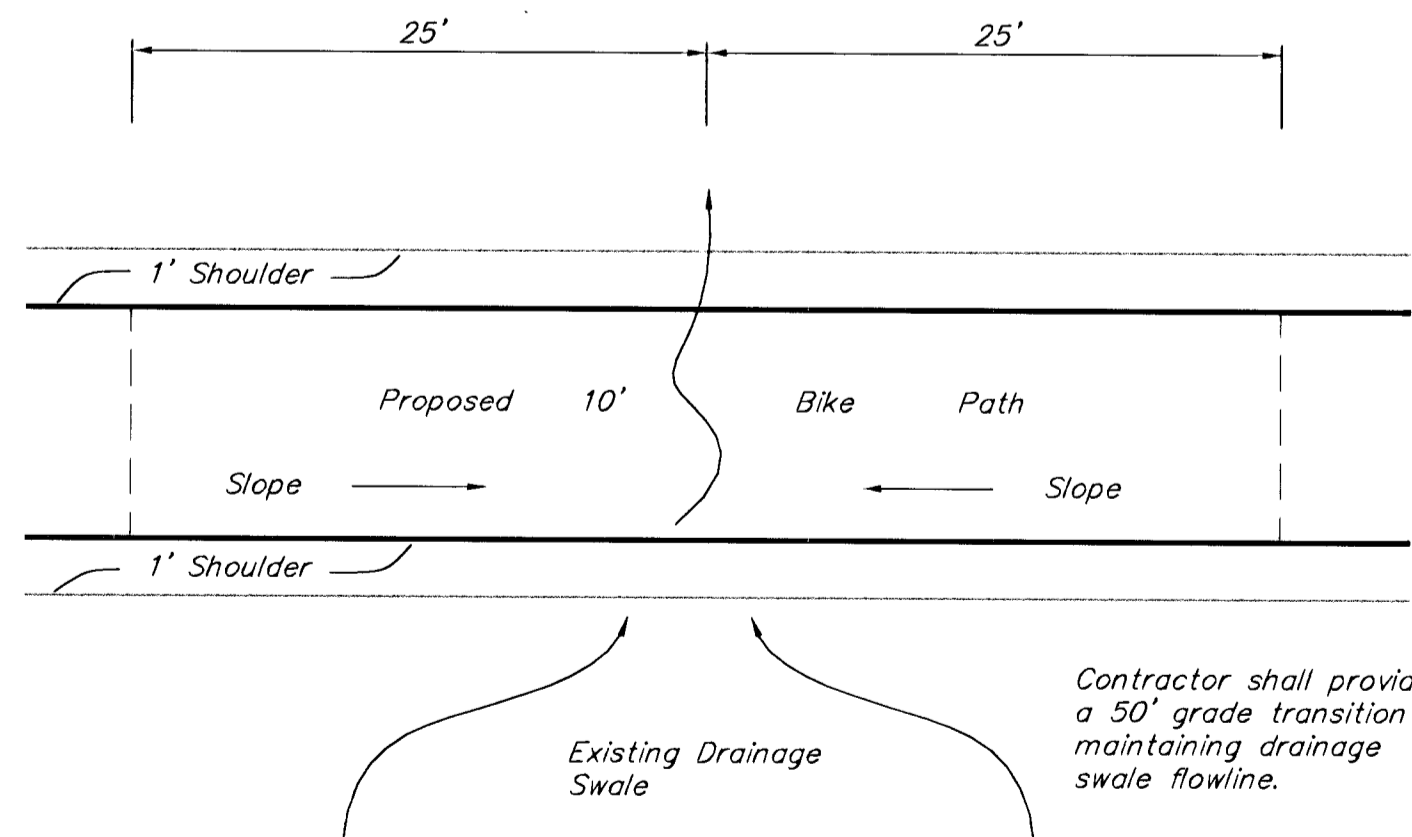


TYPICAL SECTION
NO SCALE

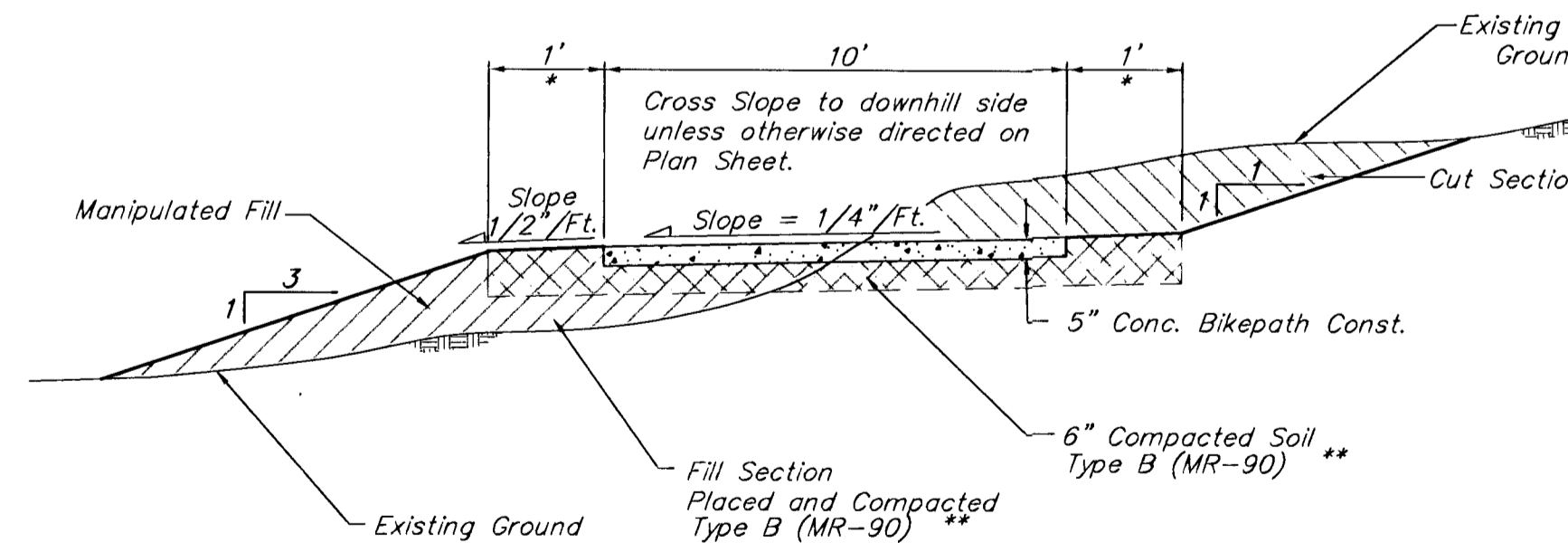


Note: Crushed Rock Base shall be paid as S.Y. 6\"/>

TYPICAL SECTION
PATH UNDER PAWNEE BRIDGE
NO SCALE



DRAINAGE SWALE
NO SCALE



TYPICAL SECTION
TRANSITION FROM TOP TO BOTTOM OF BANK
NO SCALE

CRUSHED ROCK GRADATION REQUIREMENTS
PERCENT OF AGGREGATE RETAINED

2-1/2"	0
3/4"	20 - 60
#4	50 - 80
#40	80 - 94
#200	90 - 98

ROCK QUALITY SHALL CONFORM TO THE REQUIREMENTS SPECIFIED BY THE KDOT 1990 EDITION STANDARD SPECIFICATION SUBSECTION 1102 FOR DURABILITY CLASS I.

GYPSUM CREEK BICYCLE PATH
TYPICAL SECTIONS

BAUGHMAN COMPANY, P.A.
ENGINEERING, SURVEYING, & PLANNING
316-262-7271 • 315 ELLIS • WICHITA, KANSAS 67211

PROJECT NUMBER
472-78-246-82864-000-000-001

DESIGN: JA DRAWN: BAM APPROVED: DATE: 6/96 SCALE: NONE SHEET: 2 OF 21